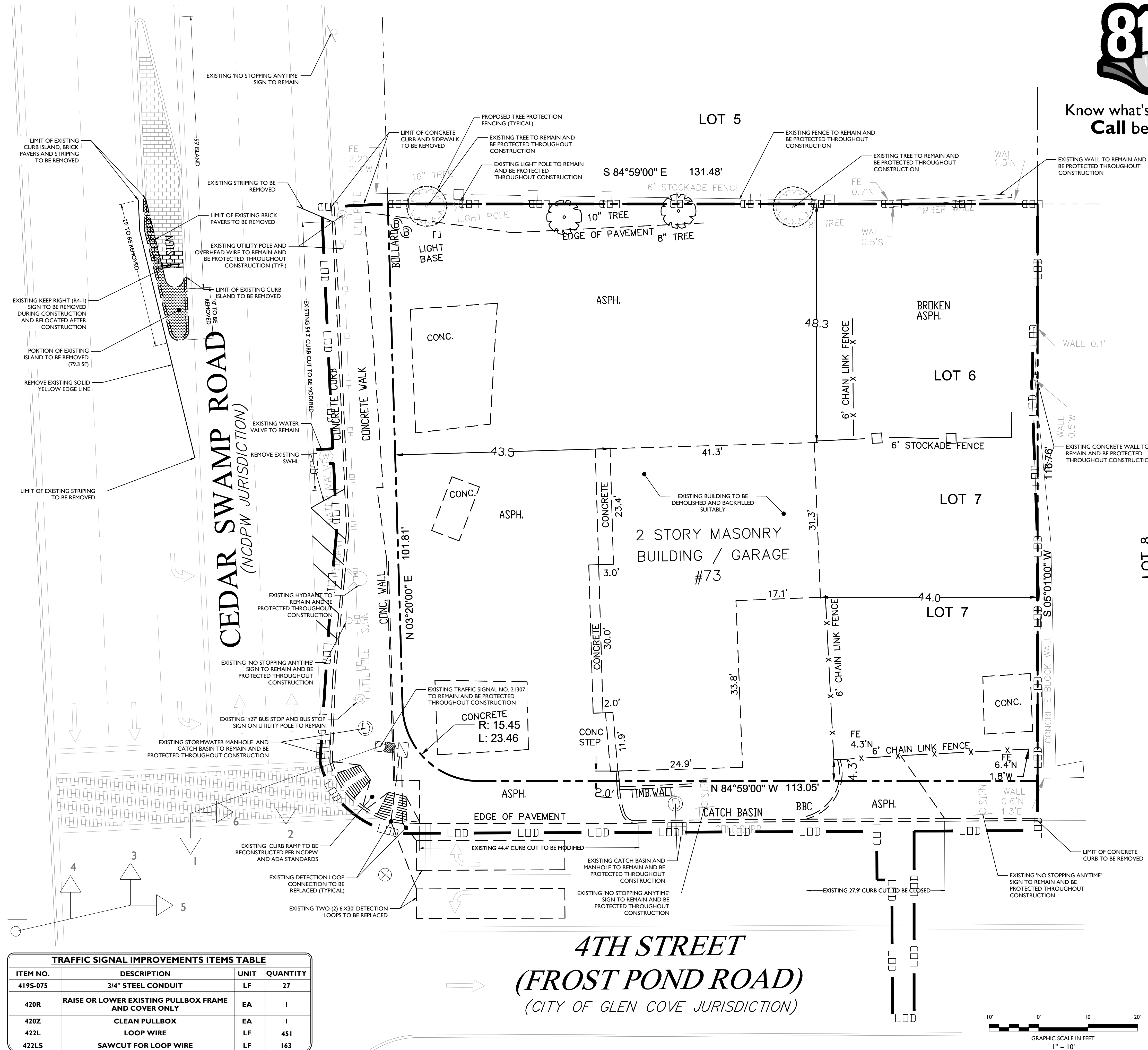




Know what's below  
Call before you dig.

SYMBOL	DESCRIPTION
---	FEATURE TO BE REMOVED / DEMOLISHED
---	LIMIT OF DISTURBANCE
---	PROPOSED TREE PROTECTION FENCE



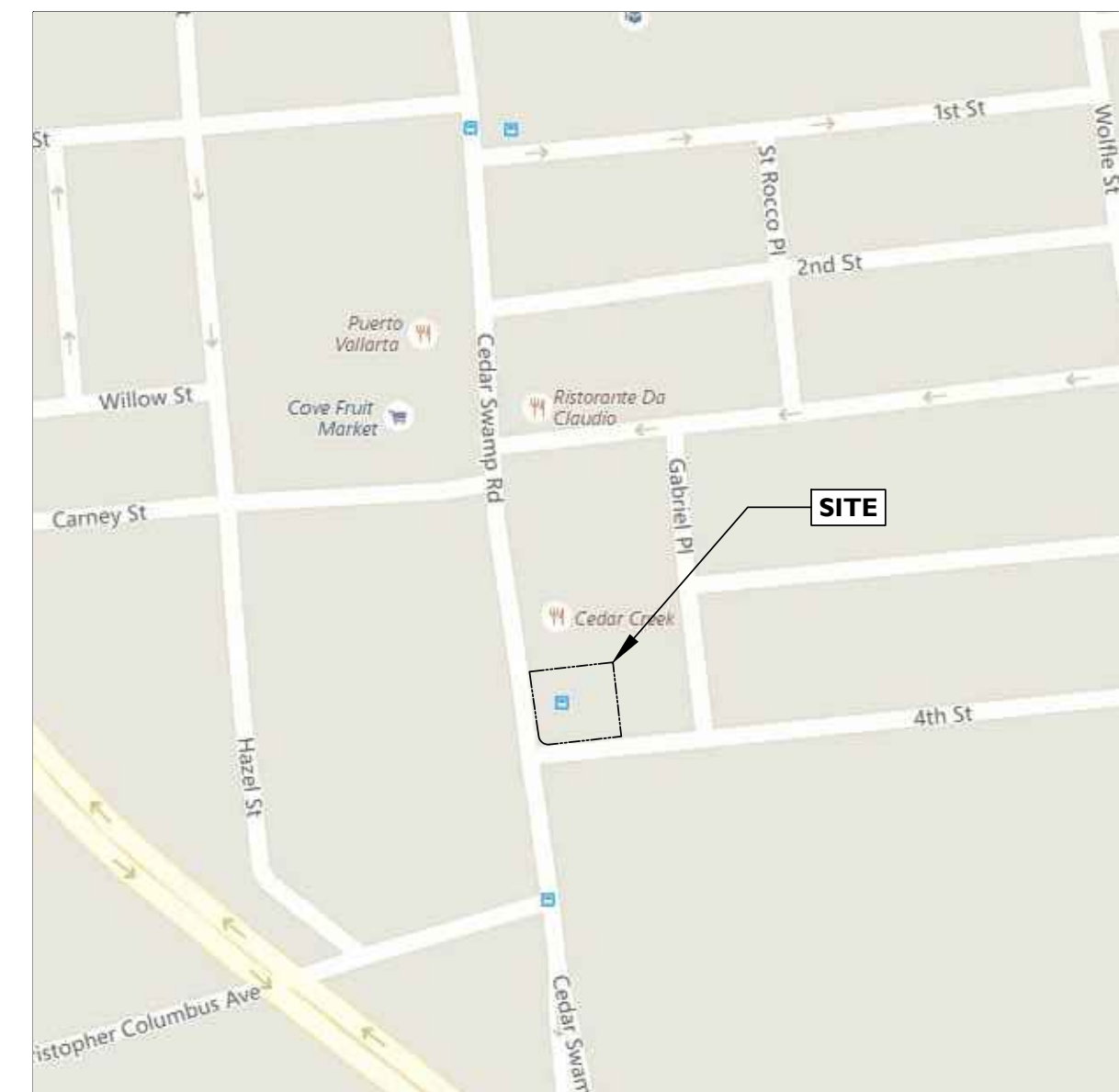
**ALL SITE FEATURES WITHIN THE LIMIT OF DISTURBANCE INDICATED ON THIS PLAN ARE TO BE REMOVED / DEMOLISHED UNLESS OTHERWISE NOTED.**

**DEMOLITION NOTES**

1. THE WORK REFLECTED ON THE DEMOLITION PLAN IS TO PROVIDE GENERAL INFORMATION TOWARDS THE EXISTING ITEMS TO BE DEMOLISHED AND/OR REMOVED. THE CONTRACTOR IS RESPONSIBLE TO REVIEW THE ENTIRE PLAN SET AND ASSOCIATED REPORTS/REFERENCE DOCUMENTS INCLUDING ALL DEMOLITION ACTIVITIES AND INCIDENTAL TASKS NECESSARY TO COMPLETE THE SITE IMPROVEMENTS.
2. THE CONTRACTOR IS RESPONSIBLE TO REPORT THE MEANS AND METHODS OF DEMOLITION ACTIVITIES.
3. EXPLOSIVES SHALL NOT BE USED UNLESS WRITTEN CONSENT FROM BOTH THE OWNER AND ANY APPLICABLE GOVERNING AGENCY IS OBTAINED. BEFORE THE START OF ANY EXPLOSIVE PROGRAM, THE CONTRACTOR IS RESPONSIBLE TO OBTAIN ALL LOCAL, STATE, AND FEDERAL PERMITS. ADDITIONALLY, THE CONTRACTOR WILL BE RESPONSIBLE FOR ALL SEISMIC TESTING AS REQUIRED AND ANY DAMAGES AS THE RESULT OF SAID DEMOLITION PRACTICES.
4. ALL DEMOLITION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL CODES. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING ALL UTILITIES ARE DISCONNECTED IN ACCORDANCE WITH THE UTILITY AUTHORITY'S REQUIREMENTS PRIOR TO STARTING THE DEMOLITION OF ANY STRUCTURE. ALL EXCAVATIONS ASSOCIATED WITH DEMOLISHED STRUCTURES OR REMOVED TANKS SHALL BE BACKFILLED WITH SUITABLE MATERIAL AND COMPACTED TO SUPPORT SITE AND BUILDING IMPROVEMENTS. A GEOTECHNICAL ENGINEER SHOULD BE PRESENT DURING BACKFILLING ACTIVITIES TO OBSERVE AND CERTIFY THAT BACKFILL MATERIAL WAS COMPACTED TO A SUITABLE CONDITION.
5. DEMOLISHED DEBRIS SHALL NOT BE BURIED ON SITE. ALL WASTE/DEBRIS GENERATED FROM DEMOLITION ACTIVITIES SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL LOCAL, STATE AND FEDERAL REQUIREMENTS. THE CONTRACTOR IS RESPONSIBLE TO MAINTAIN ALL RECORDS OF THE DISPOSAL TO DEMONSTRATE COMPLIANCE WITH THE ABOVE REGULATIONS.

**NCDPW NOTES**

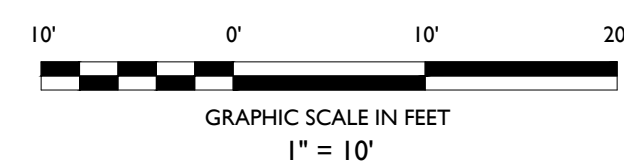
1. ALL WORK WITHIN NASSAU COUNTY RIGHT-OF-WAY SHALL CONFORM TO 2009 COUNTY OF NASSAU DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATION AND DETAIL SHEETS FOR CIVIL ENGINEERING AND SITE DEVELOPMENT CONSTRUCTION.
2. COUNTY PERMIT IS REQUIRED TO CONSTRUCT CURB, SIDEWALK, CUT EXISTING CURB, CONNECT TO COUNTY DRAINS OR FOR USE OF COUNTY PROPERTY.
3. USE OF REGULATORY SIGNS AND/OR TURN RESTRICTIONS AND/OR PARKING RESTRICTIONS ON SITE PLANS SHALL BE PLACED / INSTALLED ACCORDING TO THE LATEST "NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND THE "NEW YORK STATE SUPPLEMENT," AND SHALL BE ORDINANCE BY THE RESPECTIVE TOWN/VILLAGE/CITY.
4. THE CONTRACTOR SHALL HAVE ANY NASSAU COUNTY TRAFFIC LOOP REPLACED IF SCHEDULED TO BE DAMAGED DURING CONSTRUCTION. THE CONTRACTOR MUST HAVE AN APPROVED TRAFFIC SIGNAL CONTRACTOR PLACE THE SIGNAL IN REGALL OPERATION IN ADVANCE OF THE WORK THAT WILL DAMAGE THE LOOP DETECTOR. THE NEW LOOP WILL BE REPLACED IMMEDIATELY BY AN APPROVED TRAFFIC SIGNAL CONTRACTOR AFTER THE AREA IS RESTORED. THE SIZE AND PLACEMENT OF THE NEW LOOP WILL BE DETERMINED BY NASSAU COUNTY. A NASSAU COUNTY INSPECTOR MUST BE PRESENT UPON REPLACEMENT. THE CONTRACTOR MUST NOTIFY SHEILA M. DUKACZ, OF THE NASSAU COUNTY TRAFFIC SIGNAL CONSTRUCTION AND OPERATIONS UNIT AT 516-572-0465 EXT. 20958 IN ADVANCE OF THE WORK.



**KEY MAP**  
NOT TO SCALE

TRAFFIC SIGNAL IMPROVEMENTS ITEMS TABLE			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
419S-075	3/4" STEEL CONDUIT	LF	27
420R	RAISE OR LOWER EXISTING PULLBOX FRAME AND COVER ONLY	EA	1
420Z	CLEAN PULLBOX	EA	1
422L	LOOP WIRE	LF	451
422LS	SAWCUT FOR LOOP WIRE	LF	163

**4TH STREET  
(FROST POND ROAD)  
(CITY OF GLEN COVE JURISDICTION)**



NO.	DATE	ISSUE	DESCRIPTION
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04	04/04/2019	AR	RESUBMISSION AS PER NCDPW COMMENTS 02/14/19
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Phone 201.340.4468 · Fax 201.340.4472

**7-ELEVEN, INC.**  
PROPOSED CONVENIENCE STORE

SECTION 22, BLOCK 20, LOTS 6 & 7  
73 CEDAR SWAMP ROAD  
CITY OF GLEN COVE  
NASSAU COUNTY, NEW YORK

JEFFREY A. MARTELL, P.E.  
NEW YORK LICENSE NO. 088502  
LICENSED PROFESSIONAL ENGINEER

**STONEFIELD**  
engineering & design

SCALE: 1" = 10' PROJECT ID: T-17009

TITLE:  
**EXISTING CONDITIONS  
& DEMOLITION PLAN**

DRAWING:  
**C-1**



OFF-STREET PARKING REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§ 280-66G.(1)	REQUIRED PARKING SPACES (RETAIL BUSINESS): 1 SPACE FOR EACH 250 SF OF GFA 2,500 SF ÷ (1 (250 SF) = 10 SPACES	10 SPACES
§ 280-66G.(2)	OFF-STREET LOADING SPACES REQUIRED: 1 SPACE FOR BUILDING FLOOR AREA LESS THAN 10,000 SF	1 LOADING SPACE

SIGNAGE REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§ 228-12A	GROUND/FREESTANDING SIGN: MAXIMUM 1 SIGN PER FRONTAGE MAXIMUM HEIGHT: 15 FT MAXIMUM AREA: 24 SF MINIMUM SETBACK: 15 FT SINGLE POLE WITHOUT BRACING	1 FREESTANDING SIGN 15 FT HEIGHT 23.8 SF (SIGN #1) 15 FT SETBACK SINGLE POLE
§ 228-15	WALL SIGNS: MAXIMUM 1 SIGN PER FRONTAGE MAXIMUM HEIGHT: TOP OF BUILDING PARAPET MAXIMUM AREA: 100% WALL WIDTH (62.5 SF OR 50 SF) SIGN ILLUMINATION MUST BE EXTINGUISHED BY 9:00PM.	1 WALL SIGN DOES NOT EXTEND ABOVE TOP OF BUILDING PARAPET 44.5 SF SIGN TO BE EXTINGUISHED BY 9:00PM

**GENERAL NOTES**

- THE CONTRACTOR SHALL VERIFY AND FAMILIARIZE THEMSELVES WITH THE EXISTING SITE CONDITIONS AND THE PROPOSED SCOPE OF WORK (INCLUDING DIMENSIONS, LAYOUT, ETC.) PRIOR TO INITIATING THE IMPROVEMENTS IDENTIFIED WITHIN THESE DOCUMENTS. SHOULD ANY DISCREPANCY BE FOUND BETWEEN THE EXISTING SITE CONDITIONS AND THE PROPOSED WORK THE CONTRACTOR SHALL NOTIFY STONEFIELD ENGINEERING & DESIGN, LLC PRIOR TO THE START OF CONSTRUCTION.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND ENSURE THAT ALL REQUIRED APPROVALS HAVE BEEN OBTAINED PRIOR TO THE START OF CONSTRUCTION. COPIES OF ALL REQUIRED PERMITS AND APPROVALS SHALL BE KEPT ON SITE AT ALL TIMES DURING CONSTRUCTION.
- ALL CONTRACTORS WILL, TO THE FULLEST EXTENT PERMITTED BY LAW, INDEMNIFY AND HOLD HARMLESS STONEFIELD ENGINEERING & DESIGN, LLC AND ITS SUB-CONSULTANTS FROM AND AGAINST ANY DAMAGES AND LIABILITIES INCLUDING ATTORNEY'S FEES ARISING OUT OF CLAIMS BY EMPLOYEES OF THE CONTRACTOR IN ADDITION TO CLAIMS CONNECTED TO THE PROJECT AS A RESULT OF NOT CARRYING THE PROPER INSURANCE FOR WORKERS COMPENSATION, LIABILITY INSURANCE, AND LIMITS OF COMMERCIAL GENERAL LIABILITY INSURANCE.
- THE CONTRACTOR SHALL NOT DEVIATE FROM THE PROPOSED IMPROVEMENTS IDENTIFIED WITHIN THIS PLAN SET UNLESS APPROVAL IS PROVIDED IN WRITING BY STONEFIELD ENGINEERING & DESIGN, LLC.
- THE CONTRACTOR IS RESPONSIBLE TO DETERMINE THE MEANS AND METHODS OF CONSTRUCTION.
- THE CONTRACTOR SHALL NOT PERFORM ANY WORK OR CAUSE DISTURBANCE ON A PRIVATE PROPERTY NOT CONTROLLED BY THE PERSON OR ENTITY WHO HAS AUTHORIZED THE WORK WITHOUT PRIOR WRITTEN CONSENT FROM THE OWNER OF THE PRIVATE PROPERTY.
- THE CONTRACTOR IS RESPONSIBLE TO RESTORE ANY DAMAGED OR UNDERMINED STRUCTURE OR SITE FEATURE THAT IS IDENTIFIED TO REMAIN ON THE PLAN SET. ALL REPAIRS SHALL USE NEW MATERIALS TO RESTORE THE FEATURE TO ITS EXISTING CONDITION AT THE CONTRACTOR'S EXPENSE.
- THE CONTRACTOR IS RESPONSIBLE TO PROVIDE THE APPROPRIATE SHOP DRAWINGS, PRODUCT DATA, AND OTHER REQUIRED SUBMITTALS FOR REVIEW. STONEFIELD ENGINEERING & DESIGN, LLC WILL REVIEW THE SUBMITTALS IN ACCORDANCE WITH THE DESIGN INTENT AS REPERTED WITHIN THE PLAN SET.
- THE CONTRACTOR IS RESPONSIBLE FOR TRAFFIC CONTROL IN ACCORDANCE WITH MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- THE CONTRACTOR IS REQUIRED TO PERFORM ALL WORK IN THE PUBLIC RIGHT-OF-WAY IN ACCORDANCE WITH THE APPROPRIATE GOVERNING AUTHORITY AND SHALL BE RESPONSIBLE FOR THE PROCUREMENT OF STREET CLOSING PERMITS.
- THE CONTRACTOR IS REQUIRED TO RETAIN AN OSHA CERTIFIED SAFETY INSPECTOR TO BE PRESENT ON SITE AT ALL TIMES DURING CONSTRUCTION & DEMOLITION ACTIVITIES.
- SHOULD AN EMPLOYEE OF STONEFIELD ENGINEERING & DESIGN, LLC BE PRESENT ON SITE AT ANY TIME DURING CONSTRUCTION, IT DOES NOT RELIEVE THE CONTRACTOR OF ANY OF THE RESPONSIBILITIES AND REQUIREMENTS LISTED IN THE NOTES WITHIN THIS PLAN SET.

SYMBOL	DESCRIPTION
	SAWCUT LINE
	PROPOSED CURB
	PROPOSED DEPRESSED CURB
	PROPOSED FLUSH CURB
	PROPOSED SIGNS / BOLLARDS
	PROPOSED BUILDING
	PROPOSED CONCRETE
	PROPOSED AREA LIGHT
	PROPOSED RETAINING WALL
	PROPOSED SETBACK LINE
	PROPOSED CHAINLINK FENCE
	PROPOSED BOARD-ON-BOARD FENCE
	PROPOSED BUILDING DOORS
	PROPOSED DRIVEWAY APRON

**NASSAU COUNTY DEPARTMENT OF PUBLIC WORKS NOTES:**

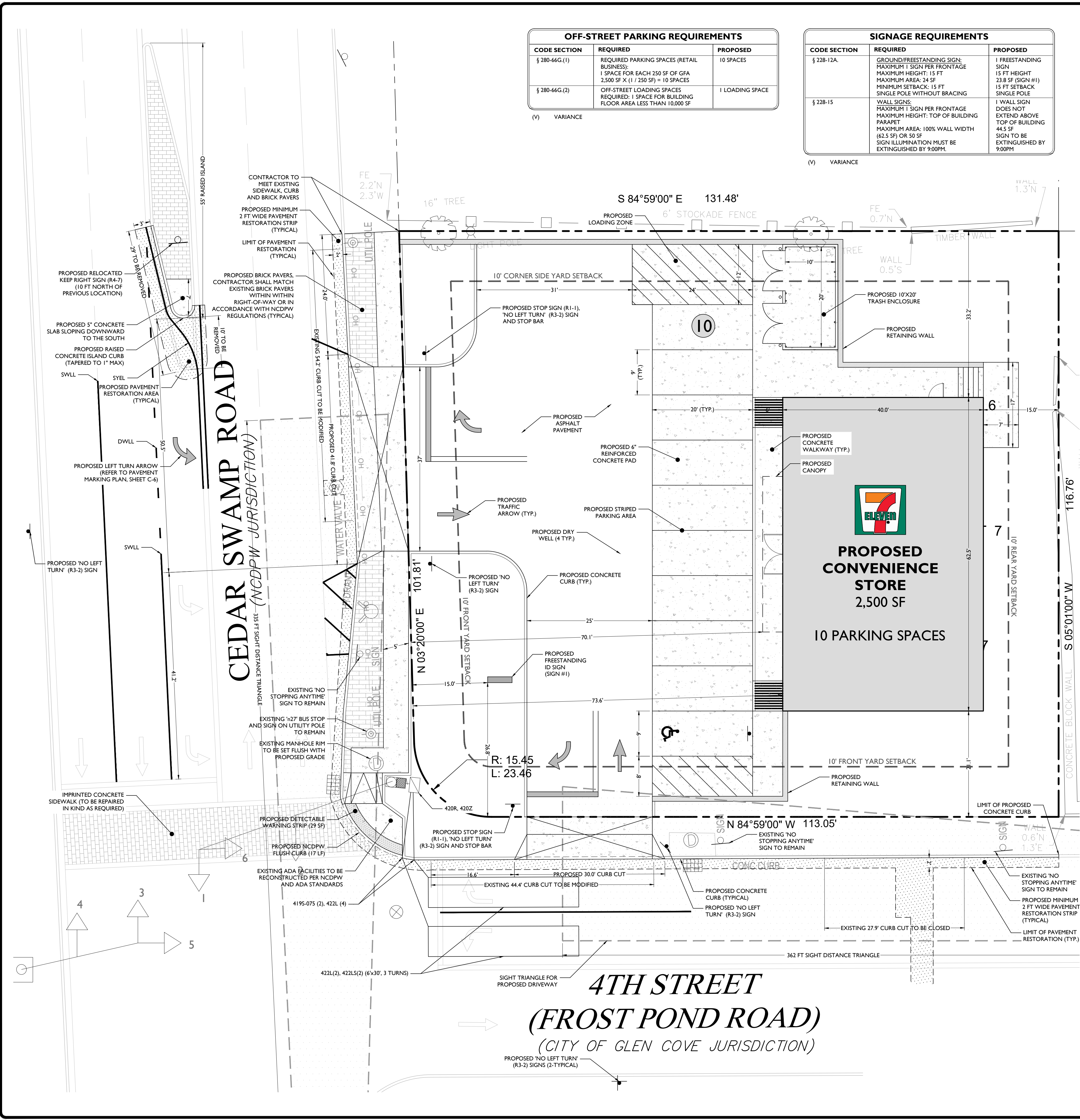
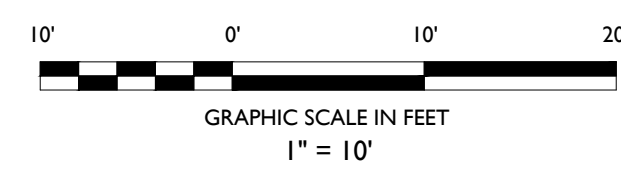
- A COUNTY PERMIT IS REQUIRED TO CONSTRUCT CURBS / SIDEWALKS.
- REPLACE ALL NON-APPROVED CURB CUTS WITH STANDARD COUNTY CURB (TYPE C). REPAIR ALL DEPRESSED, CRACKED OR MISSING SIDEWALKS, CURB(S), AND ALL NON ADA COMPLIANT RAMPS.
- REURBISH ALL PAVEMENT MARKINGS THAT HAVE BEEN REMOVED / DAMAGED DURING CONSTRUCTION, (INCLUDING BUT NOT LIMITED TO EDGE LINES, HATCHING, ETC.) ACCORDING TO NASSAU COUNTY SPECIFICATION.
- USE OF REGULATORY SIGNS AND/OR TURN RESTRICTIONS AND/OR PARKING RESTRICTIONS ON SITE PLANS SHALL BE PLACED / INSTALLED ACCORDING TO THE LATEST "NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND THE "NEW YORK STATE SUPPLEMENT," AND SHALL BE ORDINANCE BY THE RESPECTIVE TOWN/VILLAGE/CITY.
- IF NASSAU COUNTY HAS SIGNS THAT HAVE BEEN REMOVED / DAMAGED DURING CONSTRUCTION, (INCLUDING BUT NOT LIMITED TO REGULATORY SIGNS, WARNING SIGNS, ETC.) THEY SHALL BE INSTALLED / REPLACED ACCORDING TO NASSAU COUNTY SPECIFICATION.
- IF ANY NASSAU COUNTY TRAFFIC EQUIPMENT AT THE LOCATION, (INCLUDING BUT NOT LIMITED TO TRAFFIC SIGNALS, PEDESTRIAN SIGNAL EQUIPMENT, TRAFFIC LOOP WIRE DETECTION, TRAFFIC PULLBOXES, TRAFFIC SIGNAL SYSTEM INTERCONNECT, ETC.) IS DAMAGED IN ANY WAY DURING CONSTRUCTION, IT SHALL BE REPAIRED / REPLACED ACCORDING TO NASSAU COUNTY SPECIFICATION. ALL WORK SHALL BE PERFORMED BY A NASSAU COUNTY APPROVED TRAFFIC SIGNAL CONTRACTOR. THE CONTRACTOR SHALL NOTIFY SHEILA M. DUKACZ OF THE NASSAU COUNTY TRAFFIC SIGNAL CONSTRUCTION AND OPERATIONS UNIT AT 516-572-0465 EXT. 20958 IN ADVANCE OF THE WORK.
- THE CONTRACTOR SHALL REGRADE ANY NASSAU COUNTY TRAFFIC SIGNAL PULLBOX COVERS IN THE WORK AREA USING SUITABLE BRICK AND MORTAR. THE CONTRACTOR SHALL LEAVE THE INSIDE BOTTOM OF ALL PULLBOXES CLEAN AND FREE OF CONCRETE. IF ANY PULLBOX WORK IS REQUIRED, THE CONTRACTOR MUST NOTIFY SHEILA M. DUKACZ OF THE NASSAU COUNTY TRAFFIC SIGNAL CONSTRUCTION AND OPERATIONS UNIT AT 516-572-0465 EXT. 20958 IN ADVANCE OF THE WORK.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE ANY TRAFFIC SIGNAL LOOP DETECTORS THAT BECOME BROKEN BECAUSE OF ANY OF THE ENCLOSED WORK. IT IS THE CONTRACTOR'S RESPONSIBILITY TO HIRE AN APPROVED NASSAU COUNTY TRAFFIC SIGNAL CONTRACTOR TO REPLACE THE DETECTORS, TO HAVE THAT CONTRACTOR PLACE THE SIGNAL INTO RECALL OPERATION BEFORE WORK BEGINS, AND TO NOTIFY NASSAU COUNTY, SHEILA M. DUKACZ OF THE TRAFFIC SIGNAL CONSTRUCTION AND OPERATIONS UNIT AT 516-572-0465 EXT. 20958. THE SIZE AND PLACEMENT OF THE NEW LOOPS WILL BE DETERMINED BY NASSAU COUNTY. A NASSAU COUNTY INSPECTOR MUST BE PRESENT UPON REPLACEMENT OF THE LOOP DETECTORS.
- PRIOR TO CONSTRUCTION A MARK-OUT SHALL BE MADE FOR ALL UNDERGROUND UTILITIES, INCLUDING BUT NOT LIMITED TO UNDERGROUND TRAFFIC SIGNAL EQUIPMENT.

**NOTE: ALL WORK WITHIN THE NASSAU COUNTY RIGHT-OF-WAY SHALL COMPLY TO 2009 COUNTY OF NASSAU DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATIONS AND DETAIL SHEETS FOR CIVIL ENGINEERING AND SITE DEVELOPMENT CONSTRUCTION.**

TABLE OF LAND USE AND ZONING		
SECTION 22, BLOCK 020, LOTS 6 & 7		
PERIPHERAL COMMERCIAL (B-2) DISTRICT		
PROPOSED USE	CONVENIENCE STORE	PERMITTED USE
ZONING REQUIREMENT	REQUIRED	PROPOSED
MINIMUM LOT AREA	N/A	15,103 SF
MINIMUM LOT FRONTAGE	50 FT	101.81 FT
MINIMUM FRONT YARD SETBACK	10 FT	73.8 FT
MINIMUM CORNER SIDE YARD SETBACK	10 FT	21.1 FT
MINIMUM REAR YARD SETBACK	10 FT	15.0 FT
MINIMUM SIDE YARD SETBACK	0 FT OR 8 FT	33.2 FT
MAXIMUM BUILDING HEIGHT	3 STORIES / 45 FT	1 STORY / 23.0 FT
MAXIMUM LOT COVERAGE	80%	68.6% (10,371 SF)
MAXIMUM FLOOR AREA	N/A	2,500 SF

(V) VARIANCE  
(EN) EXISTING NON-CONFORMITY

**NOTE: NO WORK TO COMMENCE UNTIL A BUILDING PERMIT IS ISSUED.**



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01	07/31/2017	DD	RESUBMISSION AS PER NCDPW COMMENTS 06/02/2017
00	02/07/2017	SNO	RESUBMISSION AS PER NCDPW COMMENTS 10/20/16

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**7-ELEVEN, INC.**  
PROPOSED CONVENIENCE STORE

SECTION 22, BLOCK 20, LOTS 6 & 7  
75 CEDAR SWAMP ROAD  
CITY OF GLEN COVE  
NASSAU COUNTY, NEW YORK

**7-ELEVEN**

**STATE OF NEW YORK**  
JEFFREY A. MARTELL, P.E.  
NEW YORK LICENSE NO. 088512  
LICENSED PROFESSIONAL ENGINEER

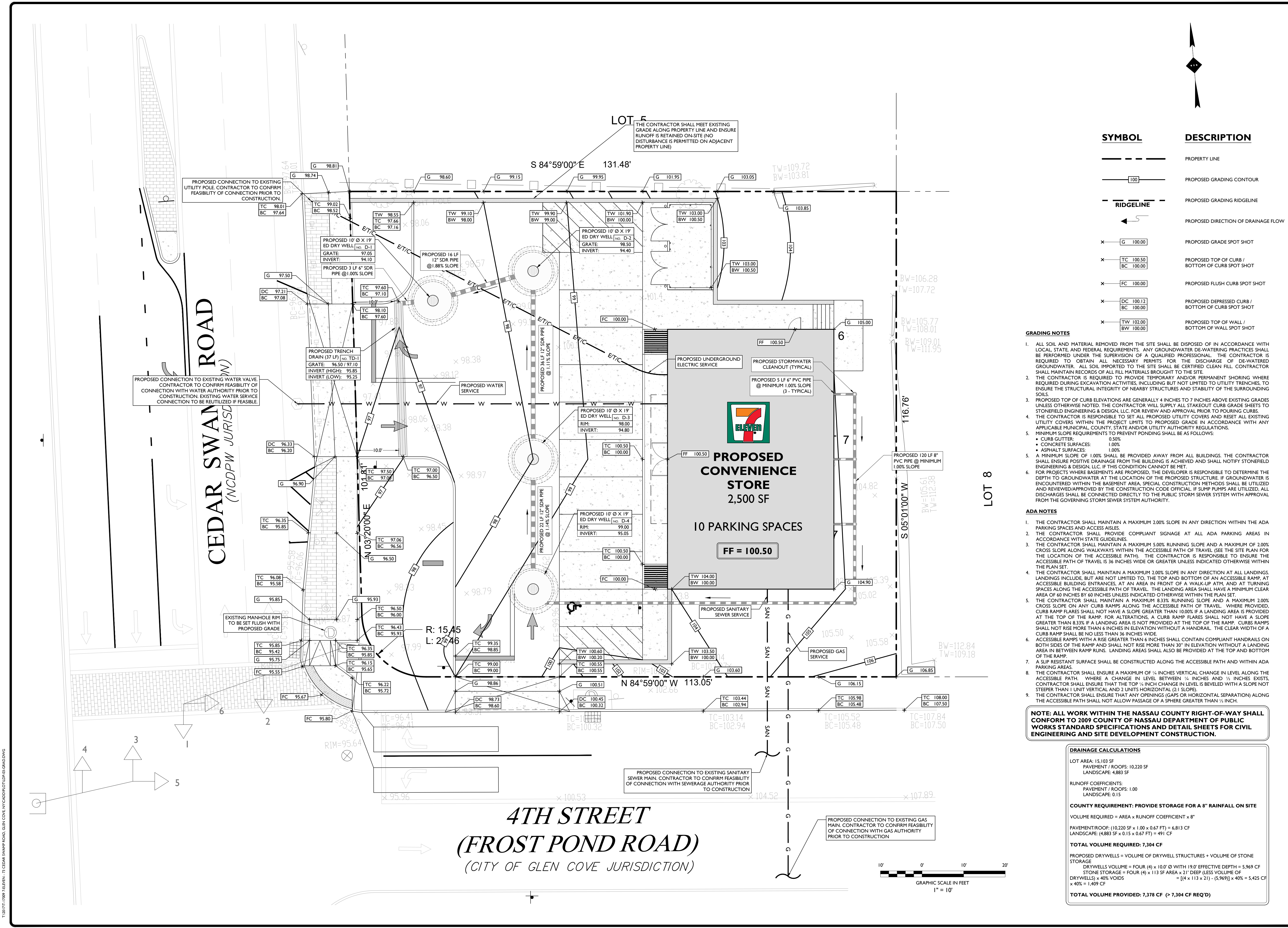
**STONEFIELD**  
engineering & design

SCALE: 1" = 10' PROJECT ID: T-17009

TITLE: **SITE PLAN**

DRAWING: **C-2**





SYMBOL	DESCRIPTION
---	PROPERTY LINE
100	PROPOSED GRADING CONTOUR
---	PROPOSED GRADING RIDGELINE
←	PROPOSED DIRECTION OF DRAINAGE FLOW
x G 100.00	PROPOSED GRADE SPOT SHOT
x TC 100.50 BC 100.00	PROPOSED TOP OF CURB / BOTTOM OF CURB SPOT SHOT
x FC 100.00	PROPOSED FLUSH CURB SPOT SHOT
x DC 100.12 BC 100.00	PROPOSED DEPRESSED CURB / BOTTOM OF CURB SPOT SHOT
x TW 102.00 BW 100.00	PROPOSED TOP OF WALL / BOTTOM OF WALL SPOT SHOT

- GRADING NOTES**
- ALL SOIL AND MATERIAL REMOVED FROM THE SITE SHALL BE DISPOSED OF IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL REQUIREMENTS. ANY GROUNDWATER DE-WATERING PRACTICES SHALL BE PERFORMED UNDER THE SUPERVISION OF A QUALIFIED PROFESSIONAL. THE CONTRACTOR IS REQUIRED TO OBTAIN ALL NECESSARY PERMITS FOR THE DISCHARGE OF DE-WATERED GROUNDWATER. ALL SOIL IMPORTED TO THE SITE SHALL BE CERTIFIED CLEAN FILL. CONTRACTOR SHALL MAINTAIN RECORDS OF ALL FILL MATERIALS BROUGHT TO THE SITE.
  - THE CONTRACTOR IS REQUIRED TO PROVIDE TEMPORARY AND/OR PERMANENT SHORING WHERE REQUIRED DURING EXCAVATION ACTIVITIES, INCLUDING BUT NOT LIMITED TO UTILITY TRENCHES, TO ENSURE THE STRUCTURAL INTEGRITY OF NEARBY STRUCTURES AND STABILITY OF THE SURROUNDING SOILS.
  - PROPOSED TOP OF CURB ELEVATIONS ARE GENERALLY 4 INCHES TO 7 INCHES ABOVE EXISTING GRADES UNLESS OTHERWISE NOTED. THE CONTRACTOR WILL SUPPLY ALL STAKEOUT CURB GRADE SHEETS TO STONEFIELD ENGINEERING & DESIGN, LLC. FOR REVIEW AND APPROVAL PRIOR TO POURING CURBS.
  - THE CONTRACTOR IS RESPONSIBLE TO SET ALL PROPOSED UTILITY COVERS AND RESET ALL EXISTING UTILITY COVERS WITHIN THE PROJECT LIMITS TO PROPOSED GRADE IN ACCORDANCE WITH ANY APPLICABLE MUNICIPAL, COUNTY, STATE AND/OR UTILITY AUTHORITY REGULATIONS.
  - MINIMUM SLOPE REQUIREMENTS TO PREVENT PONDING SHALL BE AS FOLLOWS:
    - CURB GUTTER: 0.50%
    - CONCRETE SURFACES: 1.00%
    - ASPHALT SURFACES: 1.00%
  - A MINIMUM SLOPE OF 1.00% SHALL BE PROVIDED AWAY FROM ALL BUILDINGS. THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE FROM THE BUILDING IS ACHIEVED AND SHALL NOTIFY STONEFIELD ENGINEERING & DESIGN, LLC. IF THIS CONDITION CANNOT BE MET.
  - FOR PROJECTS WHERE BASEMENTS ARE PROPOSED, THE DEVELOPER IS RESPONSIBLE TO DETERMINE THE DEPTH TO GROUNDWATER AT THE LOCATION OF THE PROPOSED STRUCTURE. IF GROUNDWATER IS ENCOUNTERED WITHIN THE BASEMENT AREA, SPECIAL CONSTRUCTION METHODS SHALL BE UTILIZED AND REVIEWED/APPROVED BY THE CONSTRUCTION CODE OFFICIAL. IF SLUMP PUMPS ARE UTILIZED, ALL DISCHARGES SHALL BE CONNECTED DIRECTLY TO THE PUBLIC STORM SEWER SYSTEM WITH APPROVAL FROM THE GOVERNING STORM SEWER SYSTEM AUTHORITY.

- ADA NOTES**
- THE CONTRACTOR SHALL MAINTAIN A MAXIMUM 2.00% SLOPE IN ANY DIRECTION WITHIN THE ADA PARKING SPACES AND ACCESS AISLES.
  - THE CONTRACTOR SHALL PROVIDE COMPLIANT SIGNAGE AT ALL ADA PARKING AREAS IN ACCORDANCE WITH STATE GUIDELINES.
  - THE CONTRACTOR SHALL MAINTAIN A MAXIMUM 5.00% RUNNING SLOPE AND A MAXIMUM OF 2.00% CROSS SLOPE ALONG WALKWAYS WITHIN THE ACCESSIBLE PATH OF TRAVEL. SEE THE SITE PLAN FOR THE LOCATION OF THE ACCESSIBLE PATH. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THE ACCESSIBLE PATH OF TRAVEL IS 36 INCHES WIDE OR GREATER UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
  - THE CONTRACTOR SHALL MAINTAIN A MAXIMUM 2.00% SLOPE IN ANY DIRECTION AT ALL LANDINGS. LANDINGS INCLUDE, BUT ARE NOT LIMITED TO, THE TOP AND BOTTOM OF AN ACCESSIBLE RAMP, AT ACCESSIBLE BUILDING ENTRANCES, AT AN AREA IN FRONT OF A WALK-UP ATM, AND AT TURNING SPACES ALONG THE ACCESSIBLE PATH OF TRAVEL. THE LANDING AREA SHALL HAVE A MINIMUM CLEAR AREA OF 60 INCHES BY 60 INCHES UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
  - THE CONTRACTOR SHALL MAINTAIN A MAXIMUM 8.33% RUNNING SLOPE AND A MAXIMUM 2.00% CROSS SLOPE ON ANY CURB RAMPS ALONG THE ACCESSIBLE PATH OF TRAVEL WHERE PROVIDED. CURB RAMP FLARES SHALL NOT HAVE A SLOPE GREATER THAN 10.00% IF A LANDING AREA IS PROVIDED AT THE TOP OF THE RAMP. FOR ALTERATIONS, A CURB RAMP FLARE SHALL NOT HAVE A SLOPE GREATER THAN 8.33% IF A LANDING AREA IS NOT PROVIDED AT THE TOP OF THE RAMP. CURBS RAMPS SHALL NOT RISE MORE THAN 6 INCHES IN ELEVATION WITHOUT A HANDRAIL. THE CLEAR WIDTH OF A CURB RAMP SHALL BE NO LESS THAN 36 INCHES WIDE.
  - ACCESSIBLE RAMPS WITH A RISE GREATER THAN 6 INCHES SHALL CONTAIN COMPLIANT HANDRAILS ON BOTH SIDES OF THE RAMP AND SHALL NOT RISE MORE THAN 30" IN ELEVATION WITHOUT A LANDING AREA IN BETWEEN RAMP RUNS. LANDING AREAS SHALL ALSO BE PROVIDED AT THE TOP AND BOTTOM OF THE RAMP.
  - A SLIP RESISTANT SURFACE SHALL BE CONSTRUCTED ALONG THE ACCESSIBLE PATH AND WITHIN ADA PARKING AREAS.
  - THE CONTRACTOR SHALL ENSURE A MAXIMUM OF 1/4 INCHES VERTICAL CHANGE IN LEVEL ALONG THE ACCESSIBLE PATH. WHERE A CHANGE IN LEVEL BETWEEN 1/4 INCHES AND 1/2 INCHES EXISTS, CONTRACTOR SHALL ENSURE THAT THE TOP 1/4 INCH CHANGE IN LEVEL IS BEVELED WITH A SLOPE NOT STEEPER THAN 1 UNIT VERTICAL AND 2 UNITS HORIZONTAL (2:1 SLOPE).
  - THE CONTRACTOR SHALL ENSURE THAT ANY OPENINGS (GAPS OR HORIZONTAL SEPARATION) ALONG THE ACCESSIBLE PATH SHALL NOT ALLOW PASSAGE OF A SPHERE GREATER THAN 1/2 INCH.

**NOTE: ALL WORK WITHIN THE NASSAU COUNTY RIGHT-OF-WAY SHALL CONFORM TO 2009 CITY OF NASSAU DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATIONS AND DETAIL SHEETS FOR CIVIL ENGINEERING AND SITE DEVELOPMENT CONSTRUCTION.**

**DRAINAGE CALCULATIONS**

LOT AREA: 15,103 SF  
 PAVEMENT / ROOFS: 10,220 SF  
 LANDSCAPE: 4,883 SF

RUNOFF COEFFICIENTS:  
 PAVEMENT / ROOFS: 1.00  
 LANDSCAPE: 0.15

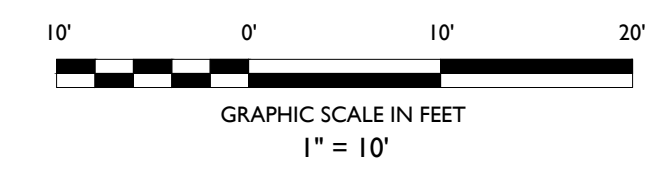
**COUNTY REQUIREMENT: PROVIDE STORAGE FOR A 8" RAINFALL ON SITE**

VOLUME REQUIRED = AREA x RUNOFF COEFFICIENT x 8"  
 PAVEMENT/ROOF: (10,220 SF x 1.00 x 0.67 FT) = 6,813 CF  
 LANDSCAPE: (4,883 SF x 0.15 x 0.67 FT) = 491 CF

**TOTAL VOLUME REQUIRED: 7,304 CF**

PROPOSED DRYWELLS = VOLUME OF DRYWELL STRUCTURES + VOLUME OF STONE STORAGE  
 DRYWELLS VOLUME = FOUR (4) x 10'0" WITH 19'0" EFFECTIVE DEPTH = 5,969 CF  
 STONE STORAGE = FOUR (4) x 113 SF AREA x 21" DEEP (LESS VOLUME OF DRYWELLS) x 40% VOIDS  
 = [(4 x 113 x 21) - (5,969) x 40% = 5,425 CF x 40% = 2,170 CF

**TOTAL VOLUME PROVIDED: 7,378 CF (> 7,304 CF REQ'D)**



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engineering & design

SCALE: 1" = 10' PROJECT ID: T-17009

TITLE:  
**GRADING, DRAINAGE & UTILITIES PLAN**

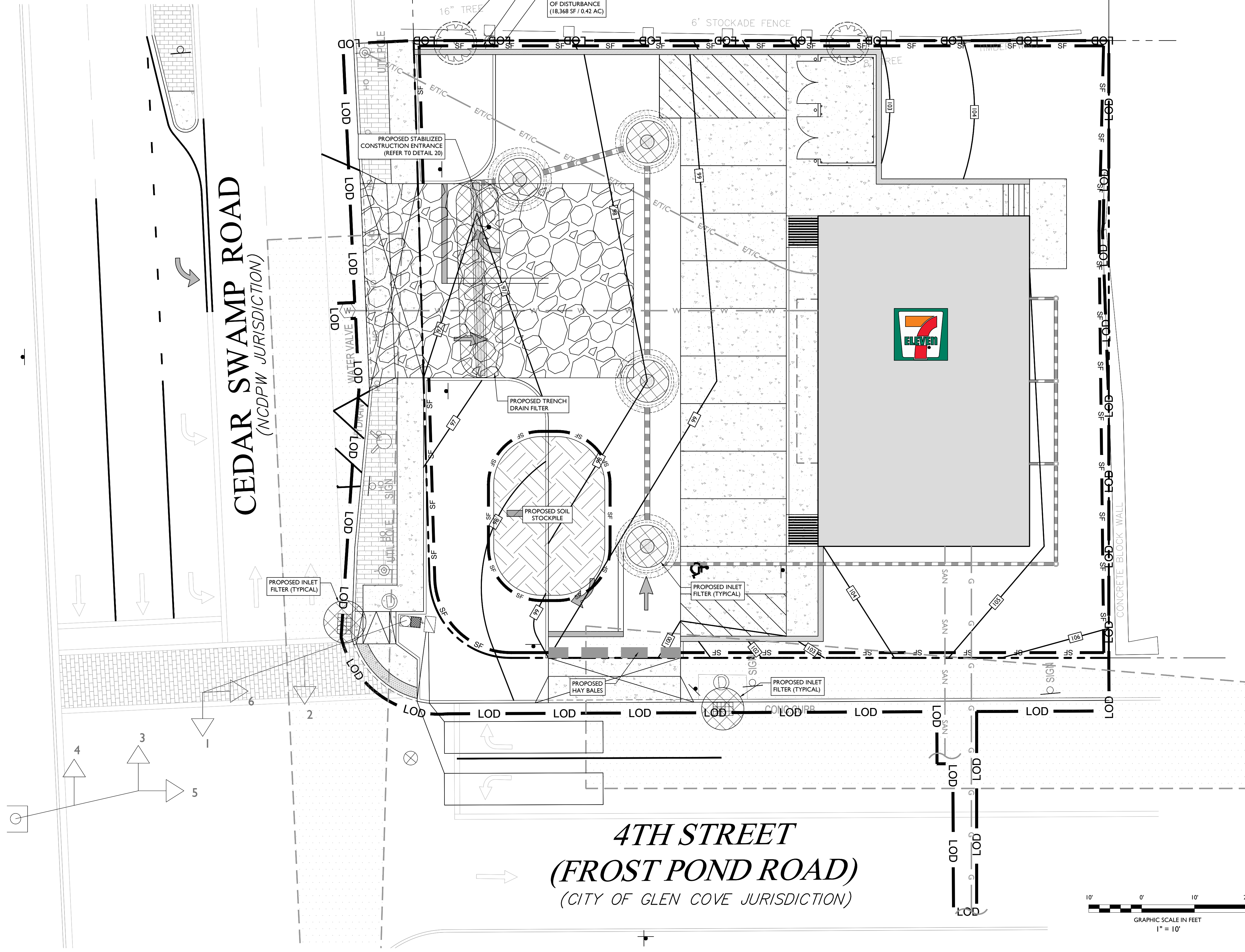
DRAWING:  
**C-3**

PROJECT: 17009 (REVISED: 7) CEDAR SWAMP ROAD, GLEN COVE, NY (CADD) PROJECT NO. 17009-03



- STABILIZATION NOTES:**
- CONTRACTOR SHALL MEET ALL GUIDELINES AND REQUIREMENTS FOR TEMPORARY STABILIZATION OF ALL DISTURBED AREAS.
  - DISTURBED AREAS SHALL BE TEMPORALLY STABILIZED WITHIN 14 DAYS OF DISTURBANCE UNLESS CONSTRUCTION SHALL RESUME IN THAT AREA WITHIN 21 DAYS.
  - CONTRACTOR SHALL MEET ALL GUIDELINES AND REQUIREMENTS FOR PERMANENT STABILIZATION OF ALL DISTURBED AREAS.

SOIL CHARACTERISTICS CHART	
TYPE OF SOIL	URBAN LAND (UIC)
PERCENT OF SITE COVERAGE	100%
HYDROLOGIC SOIL GROUP	D
DEPTH TO RESTRICTIVE LAYER	20 TO 39 INCHES
SOIL PERMEABILITY	0.00 TO 1.42 IN / HR
DEPTH TO WATER TABLE	> 37 INCHES



SYMBOL	DESCRIPTION
---	PROPERTY BOUNDARY
- - -	ADJACENT PROPERTY BOUNDARY
---	PROPOSED LIMIT OF DISTURBANCE
---	PROPOSED SILT FENCE
---	PROPOSED TREE PROTECTION FENCE
---	PROPOSED STOCKPILE & EQUIPMENT STORAGE
---	PROPOSED STABILIZED CONSTRUCTION ENTRANCE
---	PROPOSED INLET PROTECTION FILTER

**NOTE: ALL WORK WITHIN THE NASSAU COUNTY RIGHT-OF-WAY SHALL CONFORM TO 2009 COUNTY OF NASSAU DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATIONS AND DETAIL SHEETS FOR CIVIL ENGINEERING AND SITE DEVELOPMENT CONSTRUCTION.**

**EROSION AND SEDIMENT CONTROL NOTES**

- ALL SEDIMENT CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE "NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL".
- CONTRACTOR TO FOLLOW NYSDEC GUIDELINES AND REQUIREMENTS FOR CONSTRUCTION EFFORTS.
  - AFTER INITIAL SITE CLEARING, CONTRACTOR SHALL STABILIZE DISTURBED AREAS OF THE PROJECT SITE TO THE SATISFACTION OF NYSDEC REQUIREMENTS. STABILIZATION TO BE CURRENT WITH NYSDEC REQUIREMENTS AND PER THE CURRENT EDITION OF THE "NEW YORK STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL".
  - AREAS OF THE SITE THAT ARE NOT PART OF ACTIVE CONSTRUCTION EFFORT ARE TO BE IMMEDIATELY STABILIZED AFTER INITIAL SITE CLEARING AS NOTED ABOVE.
  - TIMELY MAINTENANCE OF SEDIMENT CONTROL STRUCTURES IS THE RESPONSIBILITY OF THE CONTRACTOR. ALL STRUCTURES AND DEVICES SHALL BE MAINTAINED IN GOOD WORKING ORDER AT ALL TIMES. ALL EROSION AND SEDIMENT CONTROL STRUCTURES AND DEVICES SHALL BE INSPECTED A MINIMUM OF A WEEKLY BASIS AND AFTER EACH RAINFALL EVENT OF 0.5 INCHES OR GREATER WITHIN A 24 HOUR PERIOD. FIELD BOOKS ARE TO BE MAINTAINED TO DOCUMENT COMPLIANCE WITH ALL APPLICABLE MEASURES. ALL SUPERINTENDENTS AND PROJECT TEAM MEMBERS SHALL BE PROPERLY TRAINED FOR MAINTENANCE, INSPECTION AND ENFORCEMENT OF ALL APPLICABLE PROCEDURES.
  - THE CONTRACTOR SHALL KEEP ALL ROADWAYS ADJACENT TO THE IMPROVEMENT AREA CLEAR OF SOIL AND DEBRIS.
  - TEMPORARY SEDIMENT AND EROSION CONTROL STRUCTURES SHALL BE REMOVED AFTER THE AREA OF DISTURBANCE HAS BEEN PERMANENTLY STABILIZED.
  - BEST CONSTRUCTION PRACTICES SHALL BE IMPLEMENTED TO REDUCE/ELIMINATE DUST AND DEBRIS WITHIN AND ADJACENT TO THE WORK AREAS, INCLUDING WETTING SOIL SURFACES, COVERING TRUCKS AND STORED MATERIALS WITH TARP AND PROPERLY MAINTAINING EQUIPMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FOLLOWING ALL APPLICABLE LOCAL, STATE AND FEDERAL AIR QUALITY STANDARD LAWS FOR DUST CONTROL.
  - INTERIM EROSION AND SEDIMENT CONTROL DEVICES SHALL BE INSTALLED AS NECESSARY BETWEEN CONSTRUCTION PHASES, AS WARRANTED BY CONSTRUCTION AND IN ORDER TO MINIMIZE EROSION AND TRANSPORT OF SOIL AND DEBRIS FROM THE SITE.
  - CONTRACTOR TO PREVENT SEDIMENT FROM ENTERING STORM SEWERS DURING CONSTRUCTION.
  - EXISTING ON-SITE INLETS TO BE PROVIDED WITH APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES UNTIL ON-SITE STORM SEWERS HAVE BEEN REMOVED FROM THE PROJECT SITE.
  - AREAS THAT HAVE BEEN DISTURBED ARE TO BE RE-STABILIZED AS SOON AS POSSIBLE AFTER CONSTRUCTION IS COMPLETED IN THAT AREA. TEMPORARY STABILIZATION SHALL INCLUDE, BUT NOT BE LIMITED TO, SUCH MEASURES AS HYDROSEEDING, MULCHING, JUTE MESH OR TEMPORARY PAVING AND AS ALLOWED BY THE CURRENT NYSDEC STANDARDS.
  - MISTING AND OTHER CONTROLS SHALL BE UTILIZED TO MITIGATE DUST EMISSIONS FROM THE WORK ZONE. THIS INCLUDES DURING THE DEMOLITION OF CONCRETE STRUCTURES. ALL MATERIALS Hauled FROM THE SITE SHALL BE PLACED IN LEAK PROOF CONTAINERS AND PROPERLY COVERED WITH TARPS, SCREENS AND/OR OTHER MECHANISMS.
  - A SILT FENCE SHALL BE INSTALLED ALONG THE PERIMETER OF THE SITE AND/OR AS SHOWN ON THE ACCOMPANYING PLAN. SILT FENCE SHALL BE LOCATED AT THE TOE OF ALL CRITICAL CUT AND FILL SLOPES.
  - STABILIZED CONSTRUCTION ENTRANCE SHALL BE UTILIZED BY ALL VEHICLES ACCESSING THE CONSTRUCTION SITE.

**SEQUENCE OF CONSTRUCTION:**

- OBTAIN PLAN APPROVAL AND ALL APPLICABLE PERMITS.
- HOLD PRE-CONSTRUCTION MEETING AT LEAST ONE WEEK PRIOR TO STARTING CONSTRUCTION.
- ESTABLISH CONTRACTOR STAGING AREA.
- INSTALL STABILIZED CONSTRUCTION ENTRANCE AND PERIMETER SILT FENCE / HAY BALES AS SHOWN. THESE SHALL BE THE ONLY ENTRANCES AND EXITS FOR THE CONSTRUCTION SITE.
- CONSTRUCTION AREA SECURITY FENCING MAY BE INSTALLED ALONG PERIMETER OF SITE AS LONG AS NO CONFLICTS OCCUR WITH THE REQUIREMENTS PROPOSED IN THE SEDIMENT CONTROL PLAN AND DETAILS.
- BEGIN DEMOLITION OF EXISTING STRUCTURES. ALL EXPOSED SOIL AREAS FOR DEMOLITION SHALL BE STABILIZED TEMPORARILY UNTIL NEXT PHASE OF CONSTRUCTION BEGINS.
- STOCKPILE WILL BE STABILIZED, LEVELED AND PROTECTED FROM WIND AND RUNOFF EROSION WITH PLASTIC COVERS AND PROTECTED WITH HAY BALES AND SILT FENCE.
- BEGIN ROUGH GRADING SITE.
- INSTALL PERMANENT DRAINAGE STRUCTURES AND STORMWATER CONVEYANCE SYSTEM BEGINNING FROM DOWNSTREAM WORKING UPSTREAM. INSTALL INLET FILTER PROTECTION AS REQUIRED IMMEDIATELY FOLLOWING THE INLET / CATCH BASIN BECOMING FUNCTIONAL.
- BEGIN UTILITY INSTALLATIONS AND CONNECTIONS TO PROPOSED BUILDING LOCATION.
- BEGIN SITE IMPROVEMENTS. UPON COMPLETION OF FILL ACTIVITIES, BEGIN PAVEMENT SUBGRADE PREPARATION AND CONSTRUCTION OF ON-SITE CURBING AND PAVEMENT / CONCRETE PAD BASE COURSE.
- COMPLETE FINAL GRADING FOR BUILDING.
- INSTALL ASPHALT PAVEMENT AND CONCRETE PAD.
- CONSTRUCT BUILDINGS.
- COMPLETE CONSTRUCTION / INSTALLATION OF SITE FEATURES.
- SPREAD TOPSOIL AND PERMANENTLY STABILIZE ANY PROPOSED LANDSCAPING AREAS UPSTREAM OF THE STORMWATER MANAGEMENT FACILITIES WITH SEEDING AND MULCH.
- AFTER THE SITE IS PERMANENTLY STABILIZED, REMOVE ANY REMAINING TEMPORARY SEDIMENT CONTROLS AND DEVICES. FINE GRADE AND PROVIDE STABILIZATION WITH PERMANENT SEED AND MULCH FOR ANY AREAS DISTURBED IN THIS PROCESS.

DATE	ISSUE	BY	DESCRIPTION
01/27/2020	NS		LANDSCAPING REVISIONS PER CITY COMMENTS
04/04/2019	AR		RESUBMISSION AS PER NCDPW COMMENTS 02/14/19
08/08/2018	JTJ		RESUBMISSION AS PER NCDPW COMMENTS 07/13/2018
02/16/2018	TJS		RESUBMISSION AS PER NCDPW COMMENTS 01/19/2018
07/31/2017	DD		RESUBMISSION AS PER NCDPW COMMENTS 06/02/2017
02/07/2017	SNO		RESUBMISSION AS PER NCDPW COMMENTS 10/20/2016

**NOT APPROVED FOR CONSTRUCTION**

**STONEFIELD**  
engineering & design

Rutherford, NJ · Long Island City, NY · Royal Oak, MI  
www.stonefielddesign.com

Headquarters: 92 Park Avenue, Rutherford, NJ 07070  
Phone 201.340.4468 · Fax 201.340.4472

**7-ELEVEN, INC.**  
PROPOSED CONVENIENCE STORE

SECTION 22, BLOCK 20, LOTS 6 & 7  
75 CEDAR SWAMP ROAD  
CITY OF GLEN COVE  
NASSAU COUNTY, NEW YORK

STATE OF NEW YORK  
JEFFREY A. MARTELL, P.E.  
NEW YORK LICENSE NO. 088512  
LICENSED PROFESSIONAL ENGINEER

**STONEFIELD**  
engineering & design

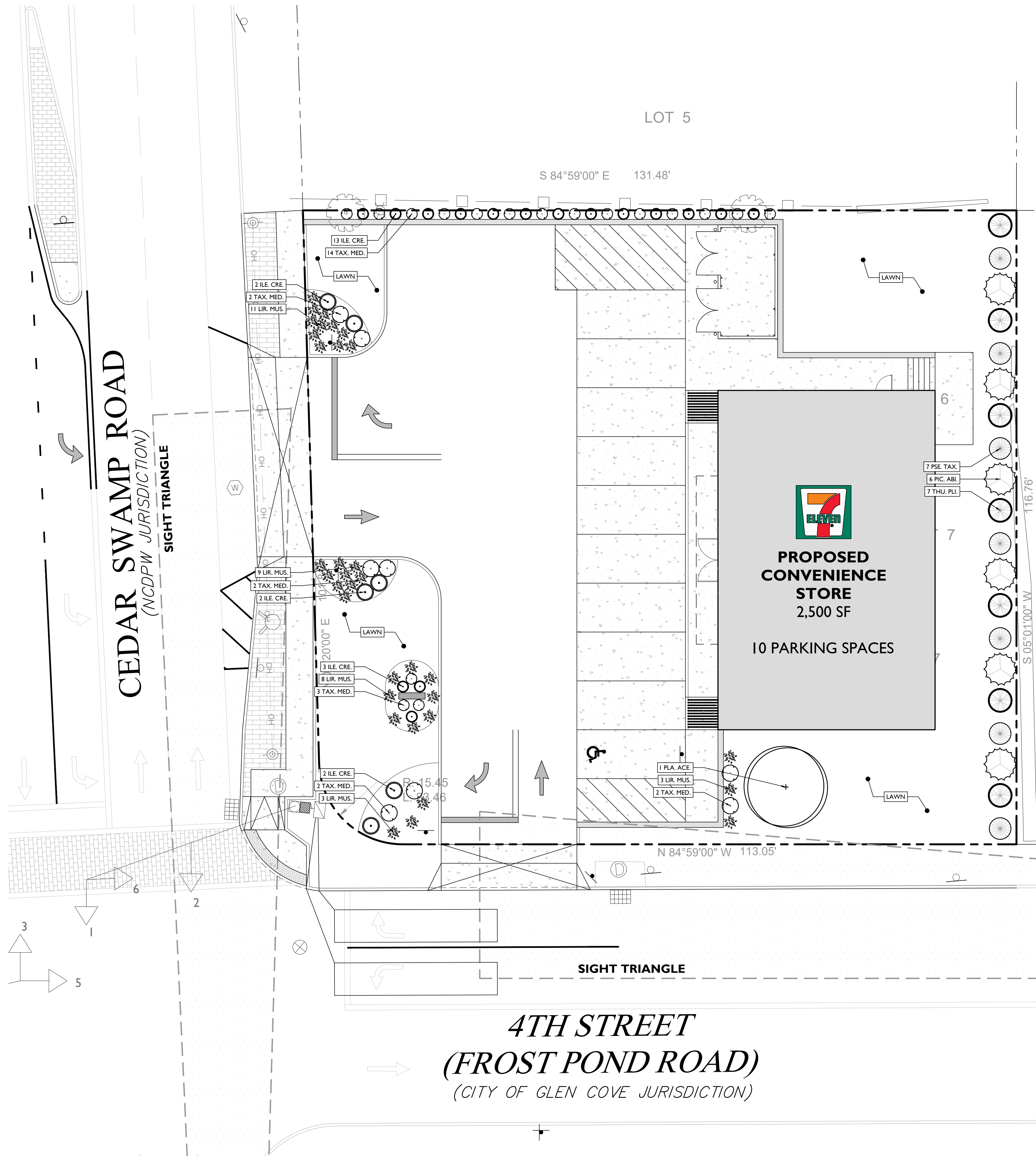
SCALE: 1" = 10' PROJECT ID: T-17009

TITLE:  
**SOIL EROSION & SEDIMENT CONTROL PLAN**

DRAWING:  
**C-4**



T:\01711-17007-7-ELEVEN-71 CEDAR SWAMP ROAD, GLEN COVE, NY\CDPW\PLANS-LANDSCAPING



PLANT SCHEDULE					
PLANT KEY	QUANTITY	BOTANICAL NAME	COMMON NAME	SIZE	MATURE SIZE
<b>DECIDUOUS TREES</b>					
PLA. ACE	1	PLATANUS ACERIFOLIA	LONDON PLANE	3.5" CAL.	HEIGHT = 50' WIDTH = 30'
<b>EVERGREEN SHRUBS</b>					
PSE. TAX.	7	PEUDOTSUGA TAXIFOLIA	DOUGLAS FIR	96" TALL	HEIGHT = 40' WIDTH = 20'
PIC. ABI.	6	PICEA ABIES	NORWAY SPRUCE	96" TALL	HEIGHT = 40' WIDTH = 20'
THU. PLI.	7	THUJA PLICATA GREEN GIANT	WESTERN ARBORVITAE	96" TALL	HEIGHT = 50' WIDTH = 15' - 20'
<b>DECIDUOUS SHRUBS</b>					
TAX. MED.	25	TAXUS X MEDIA	UPRIGHT YEW	18" O.C.	HEIGHT = 6' - 12' WIDTH = 3'
ILE. CRE.	23	ILEX CRENATA 'SKY PENCIL'	SKY PENCIL HOLLY	18" O.C.	HEIGHT = 5' - 6' WIDTH = 2'
LIR. MUS.	25	LIRIOPE MUSCARI	LILY TUFT	6" TALL	HEIGHT = 2' WIDTH = 2'

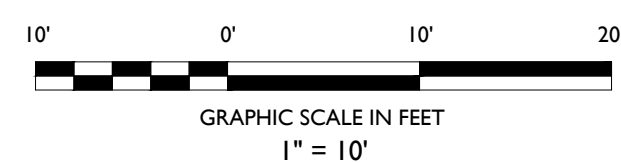
NOTE: IF ANY DISCREPANCIES OCCUR BETWEEN AMOUNTS SHOWN ON THE LANDSCAPE PLAN AND WITHIN THE PLANT LIST, THE PLAN SHALL DICTATE.

LANDSCAPING, TREE REMOVAL, AND BUFFER REQUIREMENTS		
CODE SECTION	REQUIRED	PROPOSED
§ 280-76b(8)	THE FOLLOWING STANDARD LANDSCAPING NOTES SHALL BE INCLUDED ON ALL NONRESIDENTIAL SITE PLANS UNLESS WAIVED BY THE PLANNING BOARD: (A) LANDSCAPE MATERIAL SHOWN ON THE SITE PLAN SHALL BE MAINTAINED IN VIGOROUS GROWING CONDITION CONSISTENT WITH THE INTENT OF THE PLANTING. WHERE NECESSARY TO MEET THE INTENT OF THE APPROVED SITE PLAN, DEAD PLANTS SHALL BE REPLACED WITH HEALTHY NEW PLANTS OF SIZE, TYPE AND QUALITY COMPARABLE TO THOSE SHOWN ON THE APPROVED PLAN AT THE BEGINNING OF THE NEXT GROWING SEASON. (B) ALL PLANT MATERIAL SHALL BE NURSERY GROWN AND SHALL CONFORM TO THE AMERICAN ASSOCIATION OF NURSERYMEN'S STANDARDS. (C) A CERTIFICATION SHALL BE SUBMITTED BY A QUALIFIED DESIGN PROFESSIONAL THAT ALL PLANTINGS HAVE BEEN PLANTED UNDER HIS DIRECTION AND IN ACCORDANCE WITH THE LANDSCAPING PLAN PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY. (D) THE SIZE, NUMBER AND SPECIES OF PLANTS DEPICTED ON THE SITE PLAN SHALL REPRESENT A MINIMUM STANDARD TO BE MAINTAINED. THE APPLICANT/OWNER MAY FROM TIME TO TIME ENHANCE OR MODIFY THE TYPES OF PLANTINGS USED FOR AESTHETIC REASONS, BUT THE OVERALL SIZE, QUALITY, DENSITY AND EXTENT OF THE LANDSCAPED AREAS SHALL BE MAINTAINED.	WILL COMPLY

**IRRIGATION NOTE:**  
IRRIGATION CONTRACTOR TO PROVIDE A DESIGN FOR AN IRRIGATION SYSTEM SEPARATING PLANTING BEDS FROM LAWN AREA. PRIOR TO CONSTRUCTION, DESIGN IS TO BE SUBMITTED TO THE PROJECT LANDSCAPE DESIGNER FOR REVIEW AND APPROVAL. WHERE POSSIBLE, DRIP IRRIGATION AND OTHER WATER CONSERVATION TECHNIQUES SUCH AS RAIN SENSORS SHALL BE IMPLEMENTED. CONTRACTOR TO VERIFY MAXIMUM ON SITE DYNAMIC WATER PRESSURE AVAILABLE MEASURED IN PSI. PRESSURE REDUCING DEVICES OR BOOSTER PUMPS SHALL BE PROVIDED TO MEET SYSTEM PRESSURE REQUIREMENTS. DESIGN TO SHOW ALL VALVES, PIPING, HEADS, BACKFLOW PREVENTION, METERS, CONTROLLERS, AND SLEEVES WITHIN HARDSCAPE AREAS.

- LANDSCAPING NOTES**
- THE CONTRACTOR SHALL RESTORE ALL DISTURBED GRASS AND LANDSCAPED AREAS TO MATCH EXISTING CONDITIONS UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
  - THE CONTRACTOR SHALL RESTORE ALL DISTURBED LAWN AREAS WITH A MINIMUM 4 INCH LAYER OF TOPSOIL AND SEED.
  - THE CONTRACTOR SHALL RESTORE MULCH AREAS WITH A MINIMUM 3 INCH LAYER OF MULCH.
  - THE MAXIMUM SLOPE ALLOWABLE IN LANDSCAPE RESTORATION AREAS SHALL BE 3 FEET HORIZONTAL TO 1 FOOT VERTICAL (3:1 SLOPE) UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET.
  - THE CONTRACTOR IS REQUIRED TO LOCATE ALL SPRINKLER HEADS IN AREA OF LANDSCAPING DISTURBANCE PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL RELOCATE SPRINKLER HEADS AND LINES IN ACCORDANCE WITH OWNER'S DIRECTION WITHIN AREAS OF DISTURBANCE.
  - THE CONTRACTOR SHALL ENSURE THAT ALL DISTURBED LANDSCAPED AREAS ARE GRADED TO MEET FLUSH AT THE ELEVATION OF WALKWAYS AND TOP OF CURB ELEVATIONS EXCEPT UNLESS INDICATED OTHERWISE WITHIN THE PLAN SET. NO ABRUPT CHANGES IN GRADE ARE PERMITTED IN DISTURBED LANDSCAPING AREAS.

- PLANT QUALITY AND HANDLING NOTES:**
- ALL PLANT MATERIAL SHALL CONFORM TO THE AMERICAN STANDARD FOR NURSERY STOCK (ANSI Z60.1-2004) OR LATEST REVISION AS PUBLISHED BY THE AMERICAN NURSERY AND LANDSCAPE ASSOCIATION.
  - IN ALL CASES, BOTANICAL NAMES LISTED WITHIN THE APPROVED OR FINAL PLANT LIST SHALL TAKE PRECEDENCE OVER COMMON NAMES.
  - ALL PLANTS SHALL BE OF SELECTED SPECIMEN QUALITY, EXCEPTIONALLY HEAVY, TIGHTLY KNIT, SO TRAINED OR FAVORED IN THEIR DEVELOPMENT AND APPEARANCE AS TO BE SUPERIOR IN FORM, NUMBER OF BRANCHES, COMPACTNESS AND SYMMETRY. ALL PLANTS SHALL HAVE A NORMAL HABIT OR SOUND, HEALTHY, VIGOROUS PLANTS WITH WELL DEVELOPED ROOT SYSTEM. PLANTS SHALL BE FREE OF DISEASE, INSECT PESTS, EGGS OR LARVAE.



NO.	DATE	ISSUE	DESCRIPTION
05	01/27/2020	NS	LANDSCAPING REVISIONS PER CITY COMMENTS
04	04/04/2019	AR	RESUBMISSION AS PER NCDPW COMMENTS 02/14/19
03	08/08/2018	JIT	RESUBMISSION AS PER NCDPW COMMENTS 07/13/2018
02	02/16/2018	TJS	RESUBMISSION AS PER NCDPW COMMENTS 01/19/2018
01	07/31/2017	DD	RESUBMISSION AS PER NCDPW COMMENTS 06/02/2017
00	02/07/2017	SPO	RESUBMISSION AS PER NCDPW COMMENTS 10/20/2016

**NOT APPROVED FOR CONSTRUCTION**

**STONEFIELD**  
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www.stonefielddesign.com

Headquarters: 92 Park Avenue, Rutherford, NJ 07070  
Phone 201.340.4468 · Fax 201.340.4472

**NCDPW SITE PLAN**

**7-ELEVEN, INC.**

**PROPOSED CONVENIENCE STORE**

SECTION 22, BLOCK 20, LOTS 6 & 7  
71 CEDAR SWAMP ROAD  
CITY OF GLEN COVE  
NASSAU COUNTY, NEW YORK

STATE OF NEW YORK  
JEFFREY A. MARTELL, P.E.  
NEW YORK LICENSE NO. 068512  
LICENSED PROFESSIONAL ENGINEER

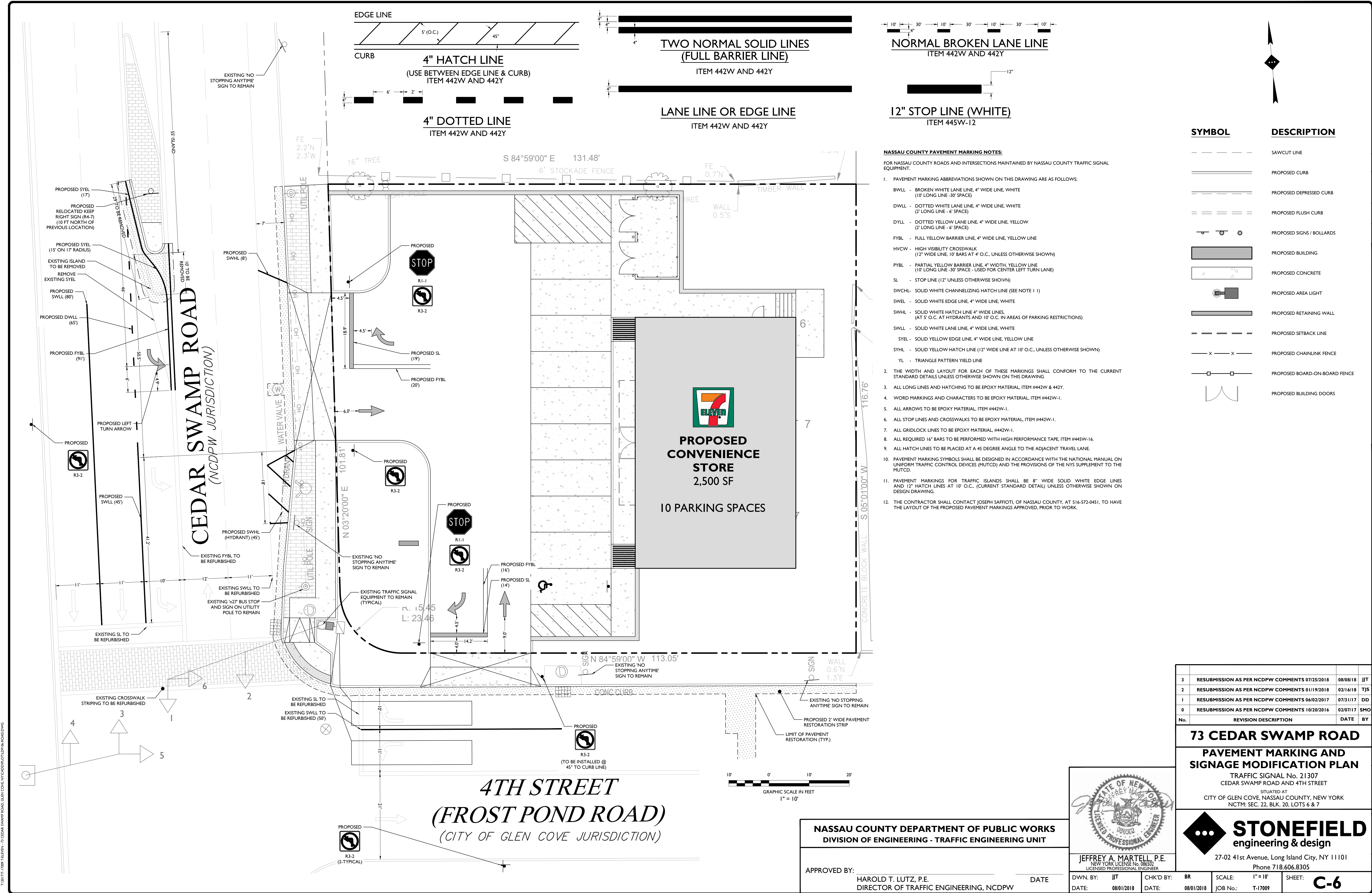
**STONEFIELD**  
engineering & design

SCALE: 1" = 10' PROJECT ID: T-17009

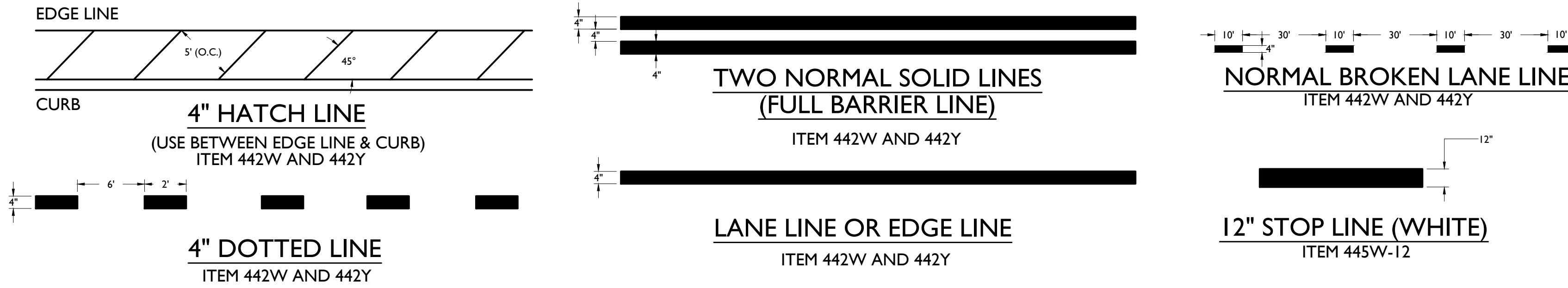
TITLE:  
**LANDSCAPING PLAN**

DRAWING:  
**C-5**





TYPICAL: 17007 TELETYPE: 71 CEDAR SWAMP ROAD, GLEN COVE, NY (CITY OF GLEN COVE JURISDICTION)



**NASSAU COUNTY PAVEMENT MARKING NOTES:**  
 FOR NASSAU COUNTY ROADS AND INTERSECTIONS MAINTAINED BY NASSAU COUNTY TRAFFIC SIGNAL EQUIPMENT.

- PAVEMENT MARKING ABBREVIATIONS SHOWN ON THIS DRAWING ARE AS FOLLOWS:  
 BWLL - BROKEN WHITE LANE LINE, 4" WIDE LINE, WHITE (10' LONG LINE - 30' SPACE)  
 DWLL - DOTTED WHITE LANE LINE, 4" WIDE LINE, WHITE (2' LONG LINE - 6' SPACE)  
 DYLL - DOTTED YELLOW LANE LINE, 4" WIDE LINE, YELLOW (2' LONG LINE - 6' SPACE)  
 FYBL - FULL YELLOW BARRIER LINE, 4" WIDE LINE, YELLOW LINE  
 HVCW - HIGH VISIBILITY CROSSWALK (12" WIDE LINE, 10' BARS AT 4" O.C., UNLESS OTHERWISE SHOWN)  
 PYBL - PARTIAL YELLOW BARRIER LINE, 4" WIDE LINE, YELLOW LINE (10' LONG LINE - 30' SPACE - USED FOR CENTER LEFT TURN LANE)  
 SL - STOP LINE (12" UNLESS OTHERWISE SHOWN)  
 SWCHL - SOLID WHITE CHANNELIZING HATCH LINE (SEE NOTE 1)  
 SWEL - SOLID WHITE EDGE LINE, 4" WIDE LINE, WHITE  
 SWHL - SOLID WHITE HATCH LINE 4" WIDE LINES (AT 5' O.C. AT HYDRANTS AND 10' O.C. IN AREAS OF PARKING RESTRICTIONS)  
 SWLL - SOLID WHITE LANE LINE, 4" WIDE LINE, WHITE  
 SYEL - SOLID YELLOW EDGE LINE, 4" WIDE LINE, YELLOW LINE  
 SYHL - SOLID YELLOW HATCH LINE (12" WIDE LINE AT 10' O.C., UNLESS OTHERWISE SHOWN)  
 YL - TRIANGLE PATTERN YIELD LINE
- THE WIDTH AND LAYOUT FOR EACH OF THESE MARKINGS SHALL CONFORM TO THE CURRENT STANDARD DETAILS UNLESS OTHERWISE SHOWN ON THIS DRAWING
- ALL LONG LINES AND HATCHING TO BE EPOXY MATERIAL, ITEM #442W & 442Y.
- WORD MARKINGS AND CHARACTERS TO BE EPOXY MATERIAL, ITEM #442W-1.
- ALL ARROWS TO BE EPOXY MATERIAL, ITEM #442W-1.
- ALL STOP LINES AND CROSSWALKS TO BE EPOXY MATERIAL, ITEM #442W-1.
- ALL GRIDLOCK LINES TO BE EPOXY MATERIAL, #442W-1.
- ALL REQUIRED 16" BARS TO BE PERFORMED WITH HIGH PERFORMANCE TAPE, ITEM #445W-16.
- ALL HATCH LINES TO BE PLACED AT A 45 DEGREE ANGLE TO THE ADJACENT TRAVEL LANE.
- PAVEMENT MARKING SYMBOLS SHALL BE DESIGNED IN ACCORDANCE WITH THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE PROVISIONS OF THE NYS SUPPLEMENT TO THE MUTCD.
- PAVEMENT MARKINGS FOR TRAFFIC ISLANDS SHALL BE 8" WIDE SOLID WHITE EDGE LINES AND 12" HATCH LINES AT 10' O.C. (CURRENT STANDARD DETAIL) UNLESS OTHERWISE SHOWN ON DESIGN DRAWING.
- THE CONTRACTOR SHALL CONTACT JOSEPH SAFFIOTI, OF NASSAU COUNTY, AT 516-572-0451, TO HAVE THE LAYOUT OF THE PROPOSED PAVEMENT MARKINGS APPROVED, PRIOR TO WORK.

SYMBOL	DESCRIPTION
---	SAWCUT LINE
---	PROPOSED CURB
---	PROPOSED DEPRESSED CURB
---	PROPOSED FLUSH CURB
○	PROPOSED SIGNS / BOLLARDS
▭	PROPOSED BUILDING
▭	PROPOSED CONCRETE
○	PROPOSED AREA LIGHT
▭	PROPOSED RETAINING WALL
---	PROPOSED SETBACK LINE
-x-x-	PROPOSED CHAINLINK FENCE
□	PROPOSED BOARD-ON-BOARD FENCE
∩	PROPOSED BUILDING DOORS

No.	REVISION DESCRIPTION	DATE	BY
3	RESUBMISSION AS PER NCDPW COMMENTS 07/25/2018	08/08/18	JJT
2	RESUBMISSION AS PER NCDPW COMMENTS 01/19/2018	02/16/18	TJS
1	RESUBMISSION AS PER NCDPW COMMENTS 06/02/2017	07/31/17	DD
0	RESUBMISSION AS PER NCDPW COMMENTS 10/20/2016	02/07/17	SMO

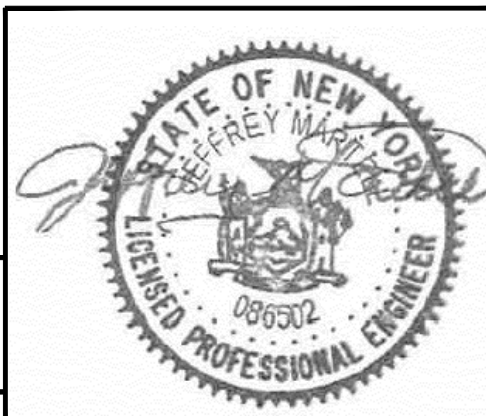
**73 CEDAR SWAMP ROAD**  
**PAVEMENT MARKING AND SIGNAGE MODIFICATION PLAN**  
 TRAFFIC SIGNAL No. 21307  
 CEDAR SWAMP ROAD AND 4TH STREET  
 SITUATED AT  
 CITY OF GLEN COVE, NASSAU COUNTY, NEW YORK  
 NCTM: SEC. 22, BLK. 20, LOTS 6 & 7



27-02 41st Avenue, Long Island City, NY 11101  
 Phone 718.606.8305

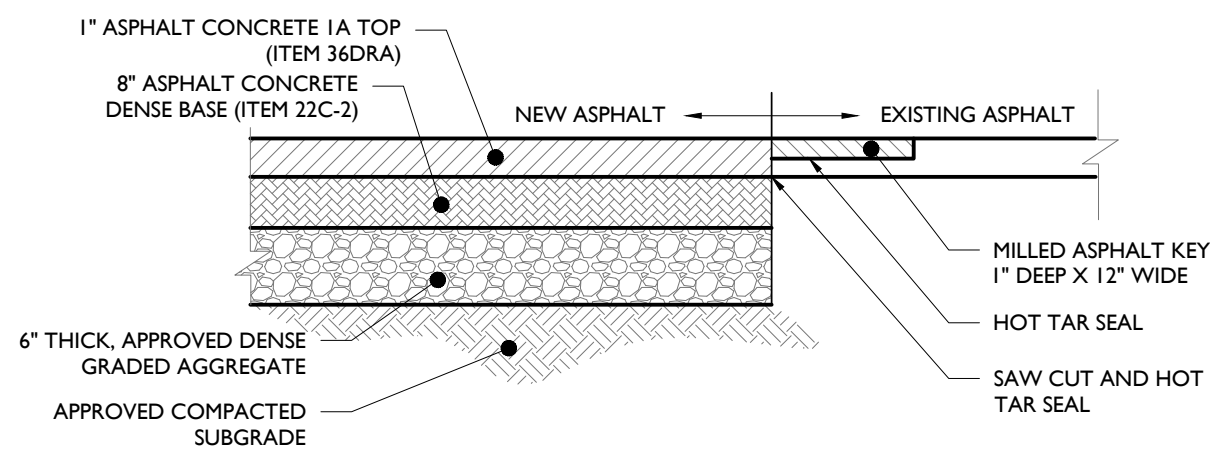
**NASSAU COUNTY DEPARTMENT OF PUBLIC WORKS**  
 DIVISION OF ENGINEERING - TRAFFIC ENGINEERING UNIT

APPROVED BY: HAROLD T. LUTZ, P.E.  
 DIRECTOR OF TRAFFIC ENGINEERING, NCDPW



DWN. BY: JT	CHK'D BY: BR	SCALE: 1" = 10'	SHEET: C-6
DATE: 08/01/2018	DATE: 08/01/2018	JOB No.: T-17009	

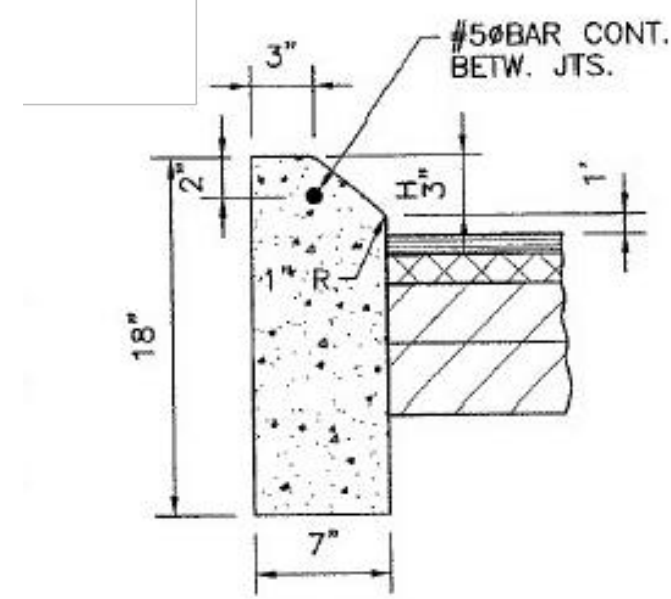




**PAVEMENT RESTORATION DETAIL**

NOT TO SCALE

- NOTES:
- HMA MIX AND DENSE GRADED AGGREGATE SHALL CONFORM TO STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
  - FOR USE WITHIN THE NASSAU COUNTY RIGHT-OF-WAY.



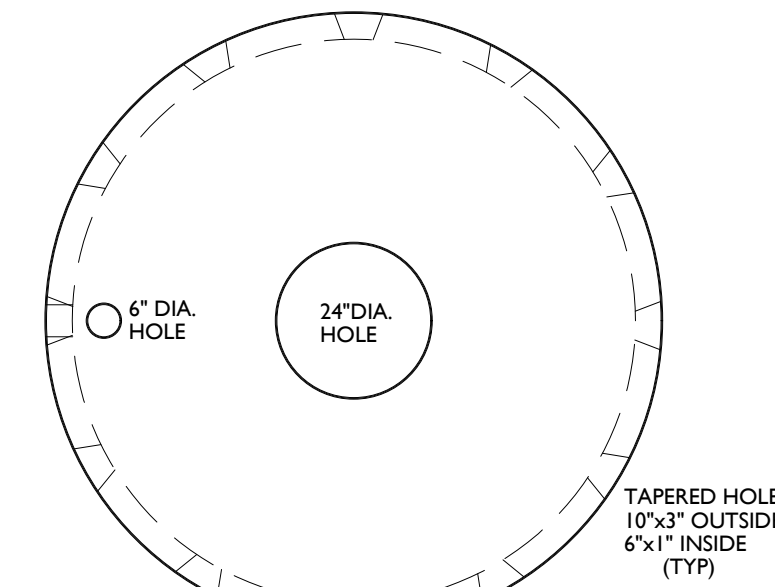
**CONCRETE CURB TYPE D  
ITEM 26SP-D**

- NOTES:
- CONCRETE SHALL BE 3500 PSI AT 28 DAYS, AIR-ENTRAINED.
  - TRANSVERSE EXPANSION JOINTS SHALL BE PROVIDED AT 20 FOOT INTERVALS WITH PRE-MOLDED, BITUMINOUS JOINT FILLER, RECESSED 1" FORM SURFACE.
  - HALF DEPTH CONTRACTION JOINTS SHALL BE PROVIDED AT 10 FOOT INTERVALS.
  - 18" CURB DEPTH SHALL BE MAINTAINED AT DEPRESSED OR FLUSH CURBED AREAS.

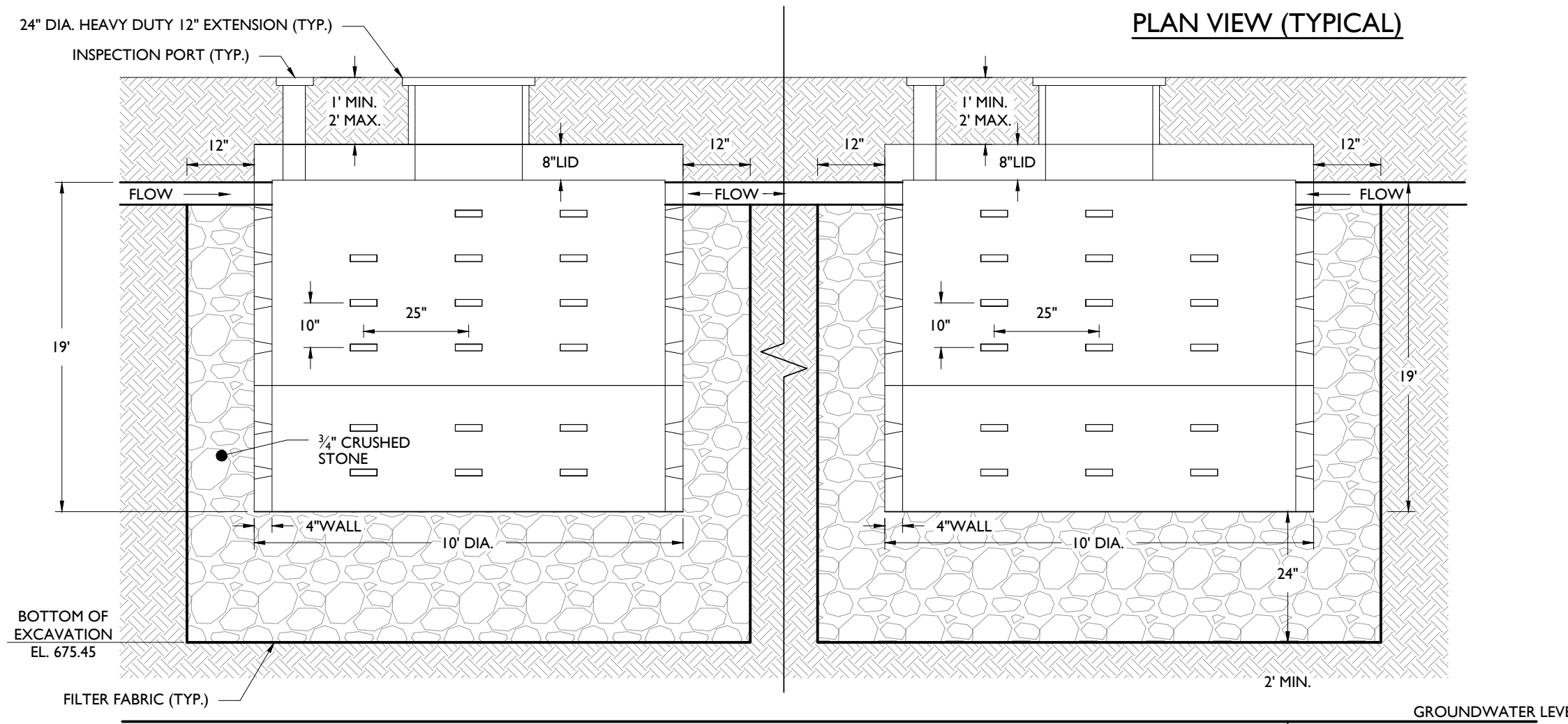
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**NOTE: ALL WORK WITHIN THE NASSAU COUNTY RIGHT-OF-WAY SHALL CONFORM TO 2009 COUNTY OF NASSAU DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATIONS AND DETAIL SHEETS FOR CIVIL ENGINEERING AND SITE DEVELOPMENT CONSTRUCTION.**

As Manufactured By  
MODERN PRECAST PRODUCTS COMPANY  
(OR APPROVED EQUAL)



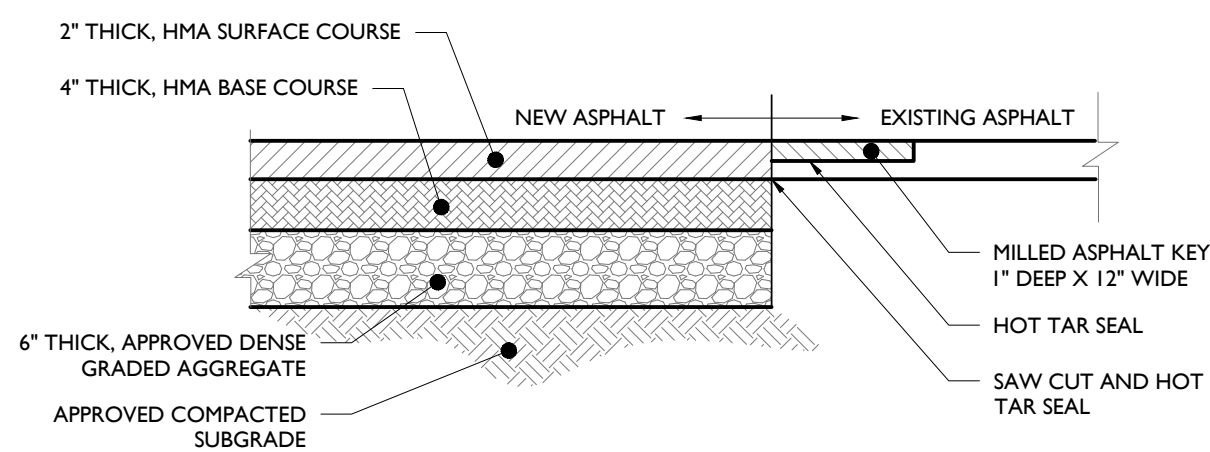
**PLAN VIEW (TYPICAL)**



**SECTION VIEW  
DRY WELL DETAIL**

NOT TO SCALE

5

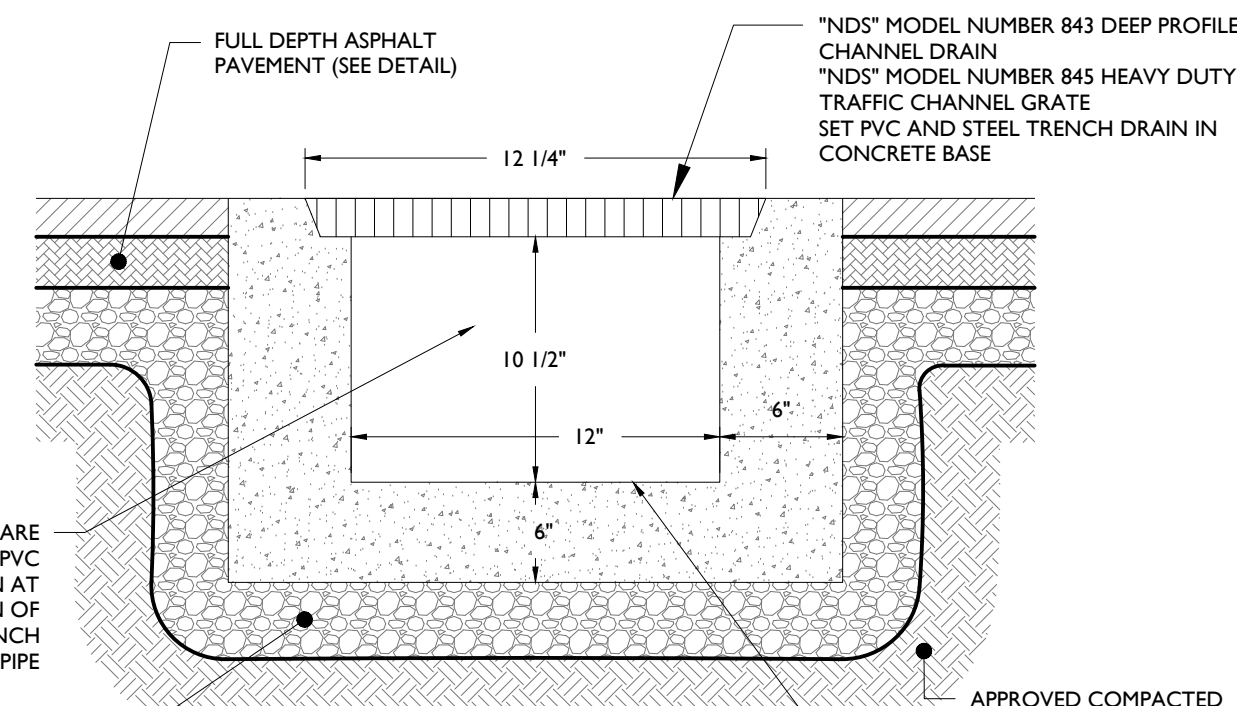


**FULL DEPTH ASPHALT PAVEMENT DETAIL**

NOT TO SCALE

- NOTES:
- HMA MIX AND DENSE GRADED AGGREGATE SHALL CONFORM TO STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS.
  - FOR USE ON SITE AND WITHIN MUNICIPAL RIGHT-OF-WAY.

3

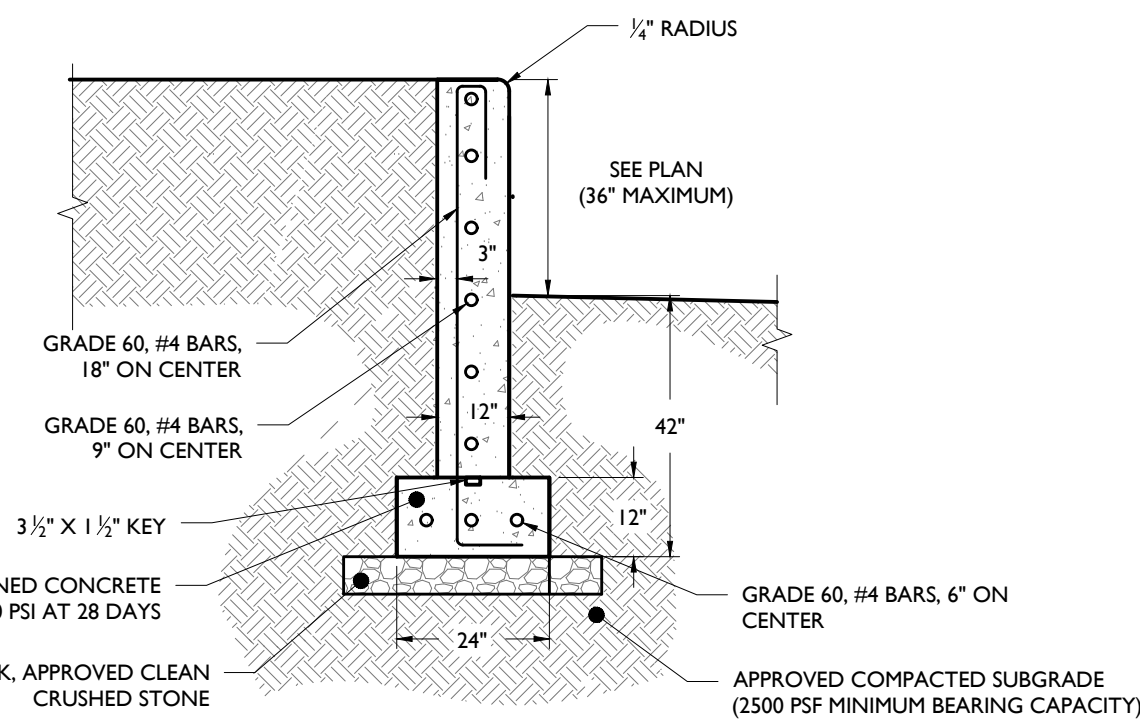


**TRENCH DRAIN**

NOT TO SCALE

- NOTE:
- SEE DRAINAGE PLAN FOR DIRECTION OF INTERNAL DRAINAGE FLOWS. INTERNAL DEPTH TO VARY TO PROVIDE 1/4" PER FOOT MINIMUM SLOPE TOWARD OUTLET.

4

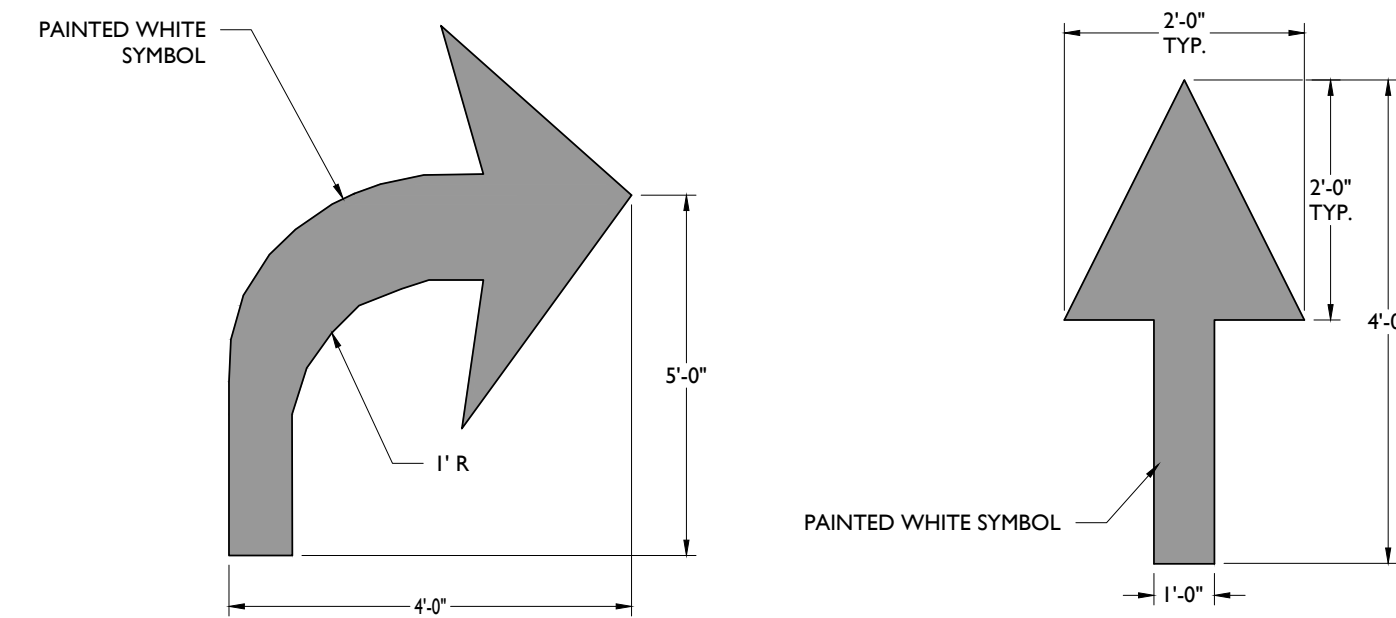


**CONCRETE RETAINING WALL DETAIL**

NOT TO SCALE

- NOTES:
- CONTRACTOR SHALL ENSURE 3" MINIMUM EMBEDMENT FOR REBAR EXPOSED TO EARTH AND 2" MINIMUM EMBEDMENT FOR REBAR EXPOSED TO WEATHER (SURFACE). NOSING REBAR TO BE EMBEDDED 1 1/2".
  - BACKFILL TO BE SUITABLE MATERIAL COMPACTED TO 95% OF MAXIMUM DENSITY IN 6" TO 8" LIFTS.

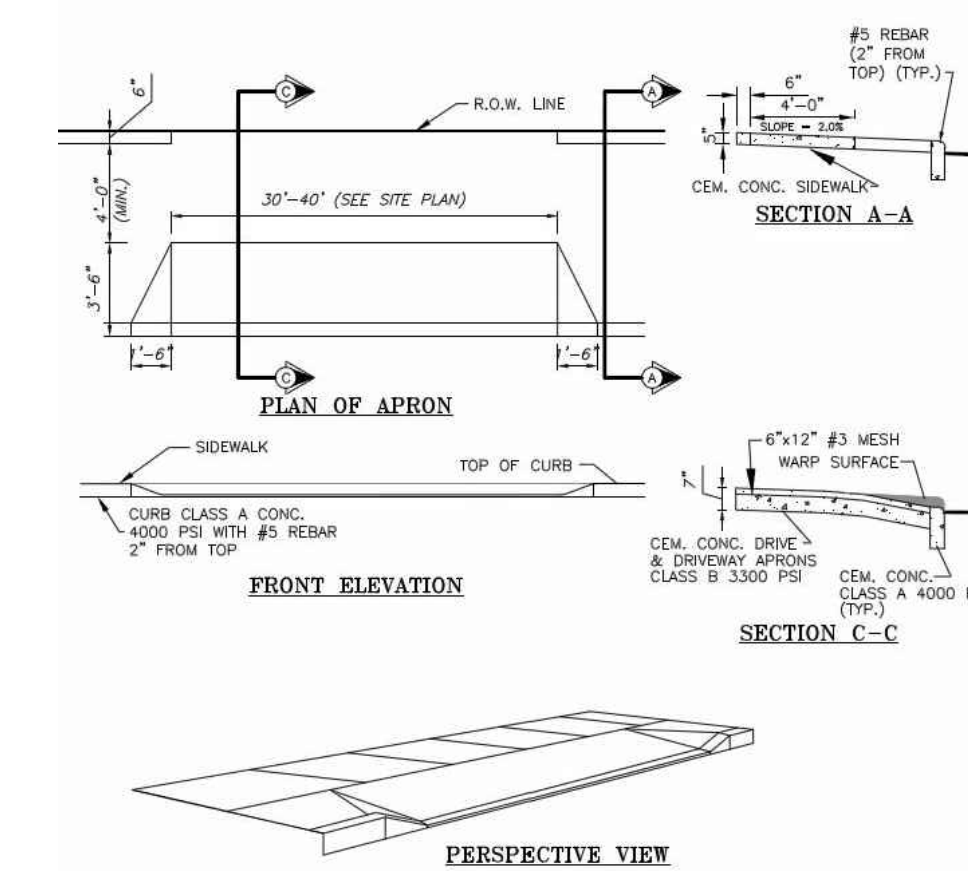
7



**ON-SITE TRAFFIC ARROW DETAILS**

NOT TO SCALE

10

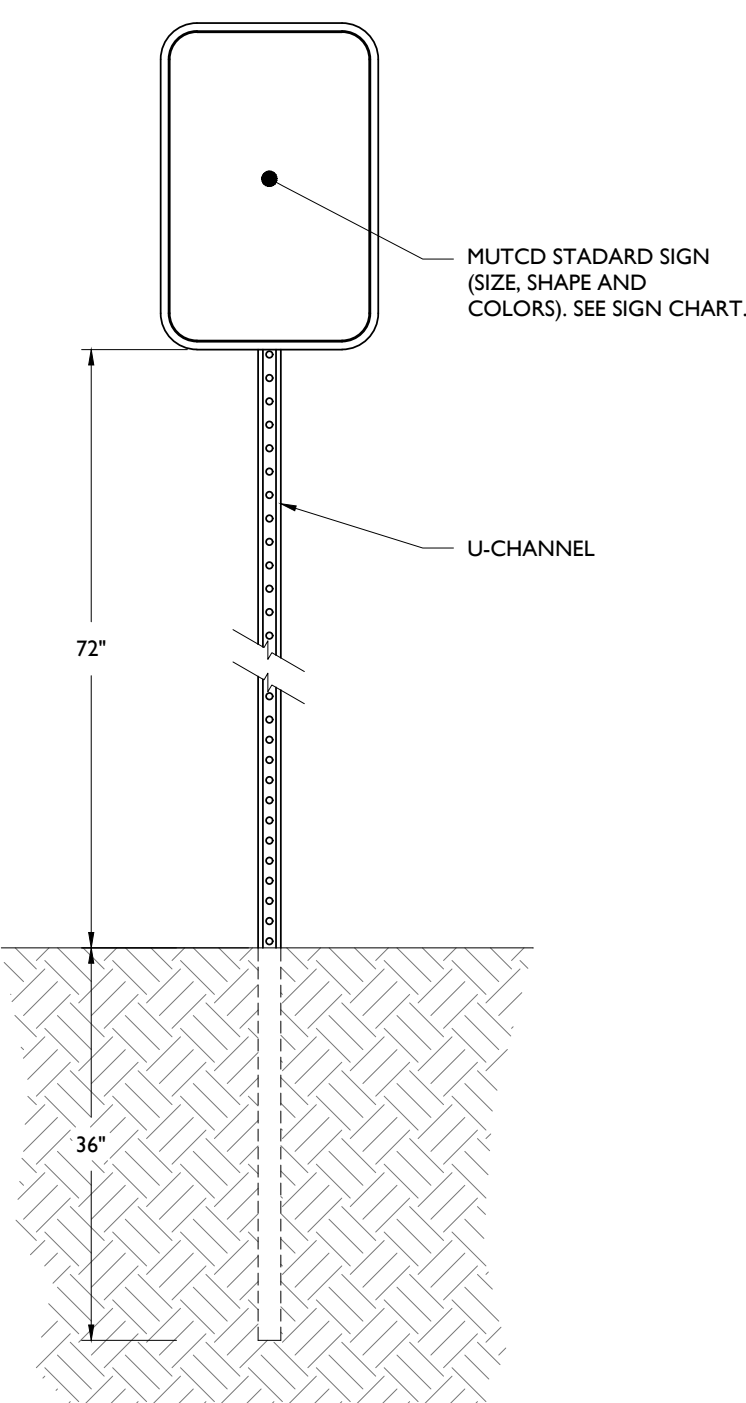


**NCDPW SIDEWALK / APRON DETAILS**

NOT TO SCALE

NOTE: FOR USE WITHIN THE NASSAU COUNTY RIGHT-OF-WAY.

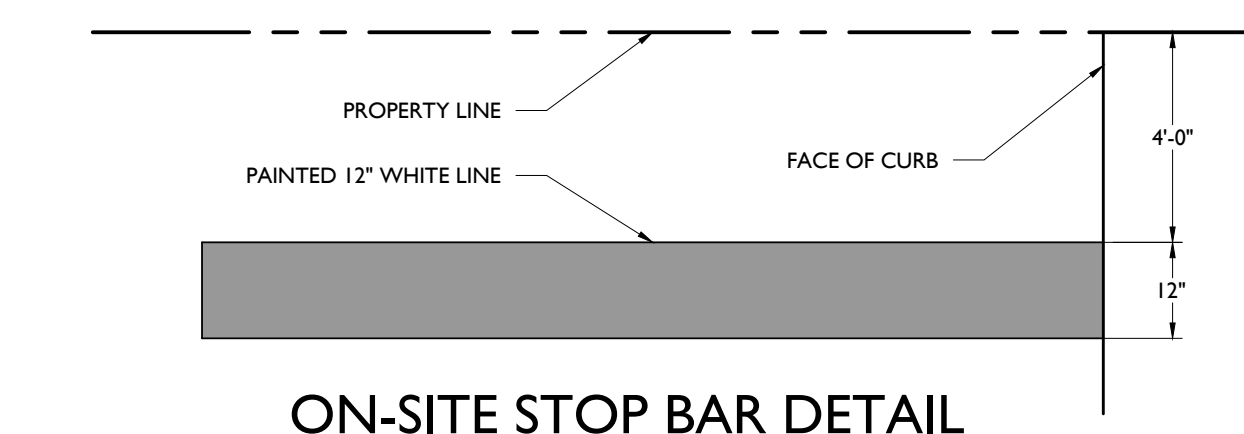
11



**SIGN POST DETAIL**

NOT TO SCALE

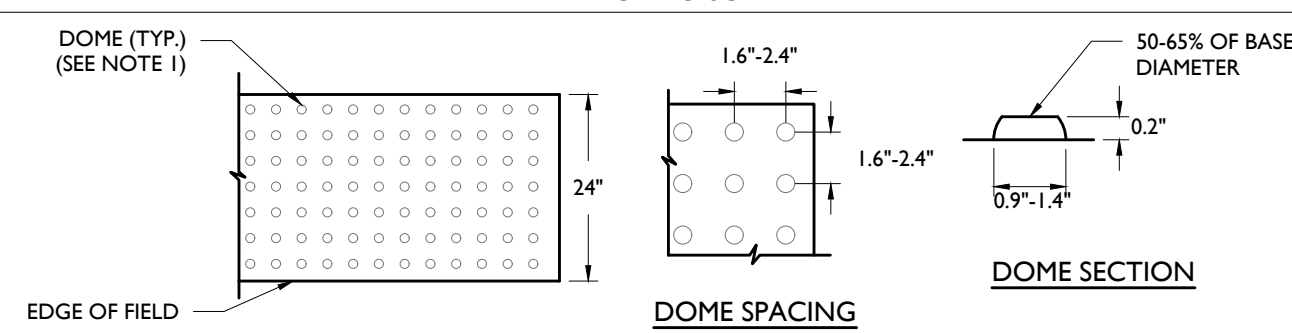
T:\03171\7009\7451\REV-7\1-CEDEM\SWAMP\ROAD\_GLEN\_COVE\_NY\CDPW\FIG\DETAIL.DWG



**ON-SITE STOP BAR DETAIL**

NOT TO SCALE

8

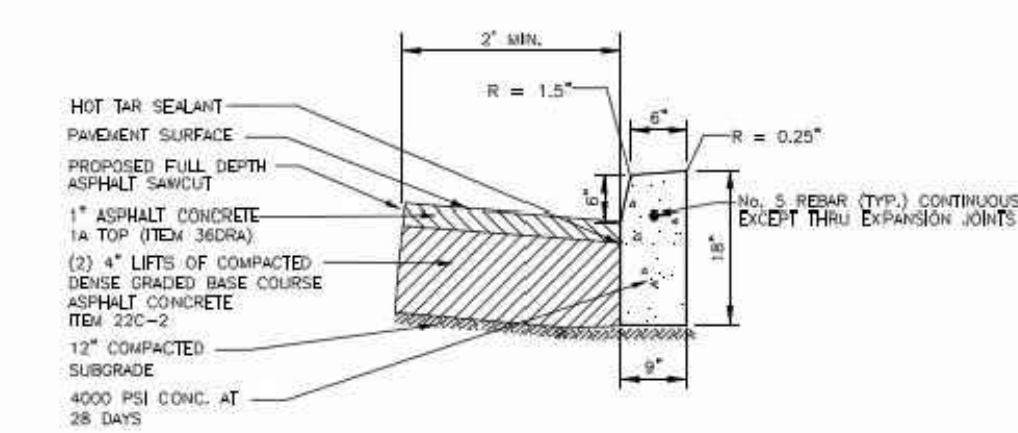


**DETECTABLE WARNING SURFACE DETAIL**

NOT TO SCALE

- NOTES:
- THE DETAILS PROVIDED ARE NOT DRAWN TO SCALE. THE QUANTITY OF DOMES DEPICTED ON THE DETECTABLE WARNING FIELD (THE DOMES AND THE ENTIRE 24" LEVEL SURFACE) IS FOR ILLUSTRATION ONLY.
  - THE SIZE OF THE DETECTABLE WARNING FIELD SHALL BE 2'-0" IN THE DIRECTION OF TRAVEL AND SHALL EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE, EXCLUSIVE OF SIDE FLARES.
  - THE ROWS OF DOMES SHALL BE ALIGNED TO BE PARALLEL TO TRAVEL DIRECTION.
  - WHERE DOMES ARE ARRANGED RADIALLY THEY MAY DIFFER IN DOME DIAMETER AND CENTER-TO-CENTER SPACING WITHIN THE RANGES SPECIFIED ON THIS SHEET.
  - DETECTABLE WARNING FIELD SHALL VISUALLY CONTRAST FROM ADJACENT WALKING SURFACE (DARK ON LIGHT OR LIGHT ON DARK).
  - DETECTABLE WARNINGS SHALL BE LOCATED SO THAT THE EDGE OR ON CURB RAMP TYPE I AT LEAST ONE CORNER OF THE WARNING FIELD NEAREST TO THE ROADWAY IS 6" TO 8" FROM THE FRONT OF CURB OR THE EDGE.

9

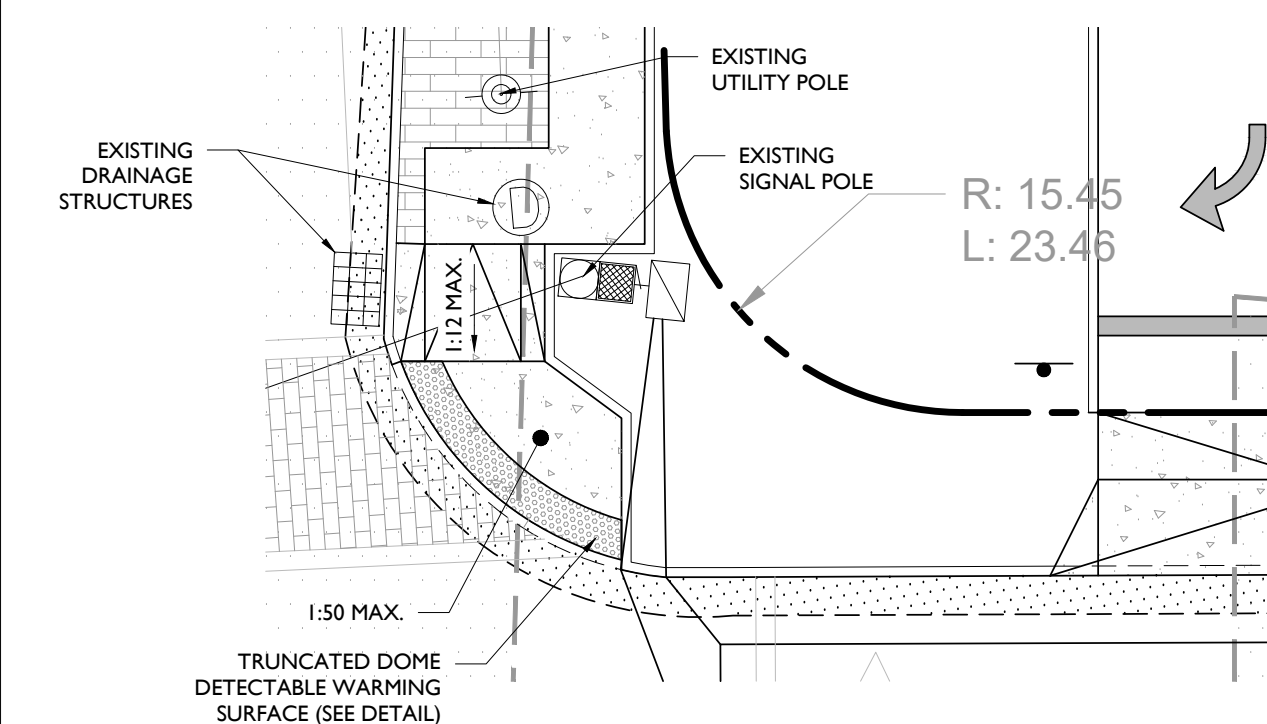


**TYPE C CONCRETE CURB (ITEM 26C) &  
PAVEMENT REPAIR DETAIL**

NOT TO SCALE

NOTE: FOR USE WITHIN THE NASSAU COUNTY RIGHT-OF-WAY.

12



**OFF-SITE CURB RAMP DETAIL**

NOT TO SCALE

13

NO.	DATE	ISSUE	BY	DESCRIPTION
05	01/27/2019	NS		LANDSCAPING REVISIONS PER CITY COMMENTS
04	04/04/2019	AR		RESUBMISSION AS PER NCDPW COMMENTS 02/14/19
03	08/08/2018	JTJ		RESUBMISSION AS PER NCDPW COMMENTS 07/13/2018
02	02/16/2018	TJS		RESUBMISSION AS PER NCDPW COMMENTS 01/19/2018
01	07/31/2017	DD		RESUBMISSION AS PER NCDPW COMMENTS 06/02/2017
00	02/07/2017	SNO		RESUBMISSION AS PER NCDPW COMMENTS 10/20/2016

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NASSAU COUNTY, NEW YORK

STATE OF NEW YORK  
JEFFREY A. MARTELL  
NEW YORK LICENSE NO. 088502  
LICENSED PROFESSIONAL ENGINEER

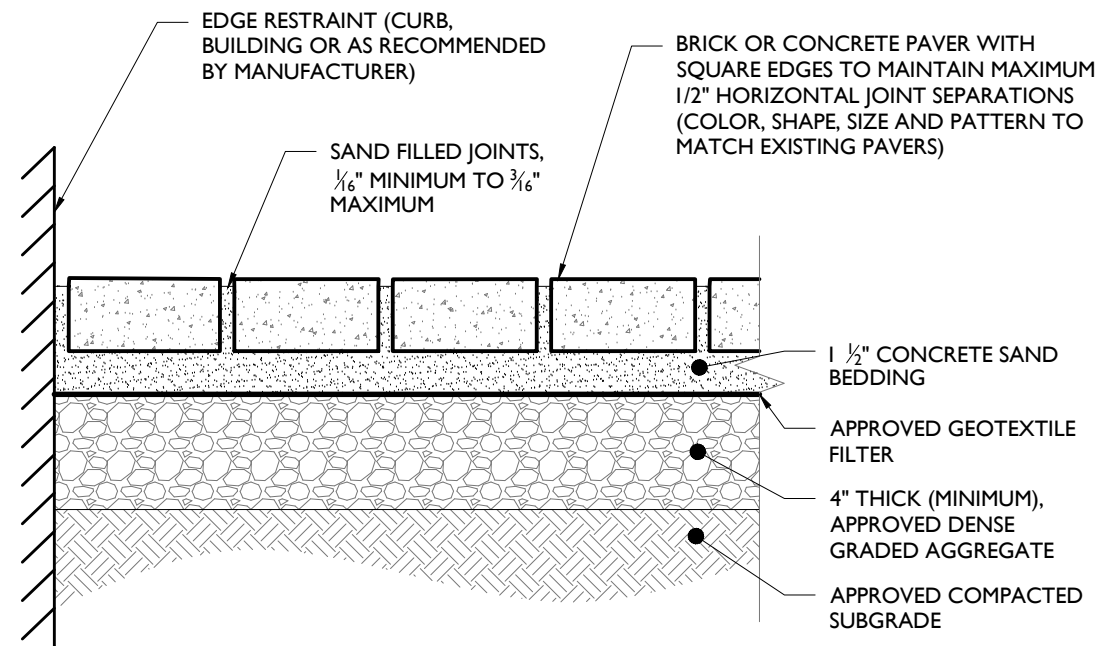
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SCALE: AS SHOWN PROJECT ID: T-17009

TITLE:  
**CONSTRUCTION  
DETAILS I**

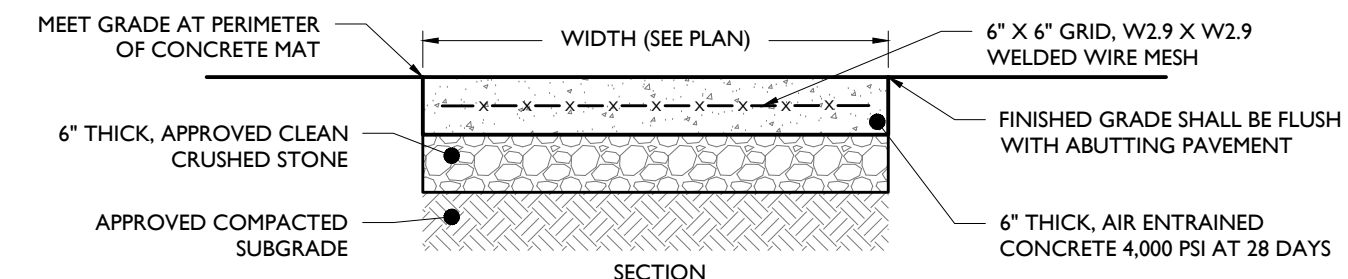
DRAWING:  
**C-7**





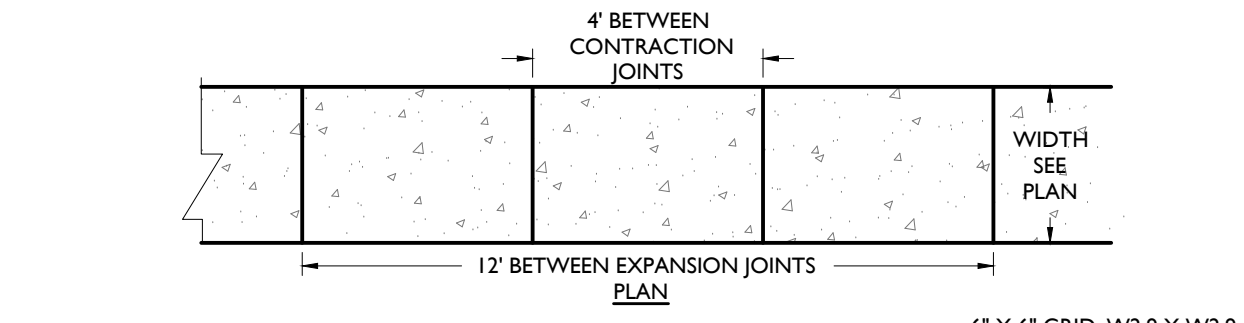
**BRICK PAVER WALKWAY DETAIL**

NOT TO SCALE



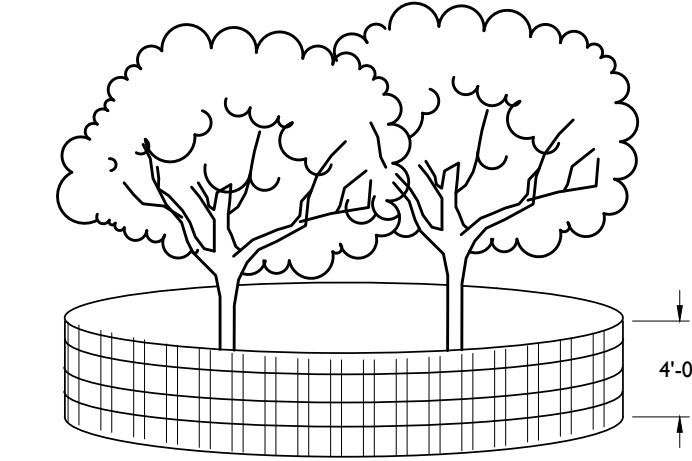
**REINFORCED 6\"/>**

NOT TO SCALE  
 NOTES:  
 1. 1/2" EXPANSION JOINTS WITH WATER SEAL SHALL BE PROVIDED AT 12' INTERVALS WITH PRE-MOLDED, BITUMINOUS JOINT FILLER, RECESSED 1/4" FROM THE SURFACE. LONGITUDINAL REBAR TO BE CUT AT EXPANSION JOINTS.  
 2. 1" DEEP BY 1/2" WIDE TOOLED CONTRACTION JOINTS SHALL BE PROVIDED AT MID-POINT BETWEEN EXPANSION JOINTS OR 6' INTERVALS MAX.  
 3. CONCRETE SHALL RECEIVE BROOM FINISH.  
 4. ALL EXPOSED CORNERS TO HAVE 1/2" CHAMFER.



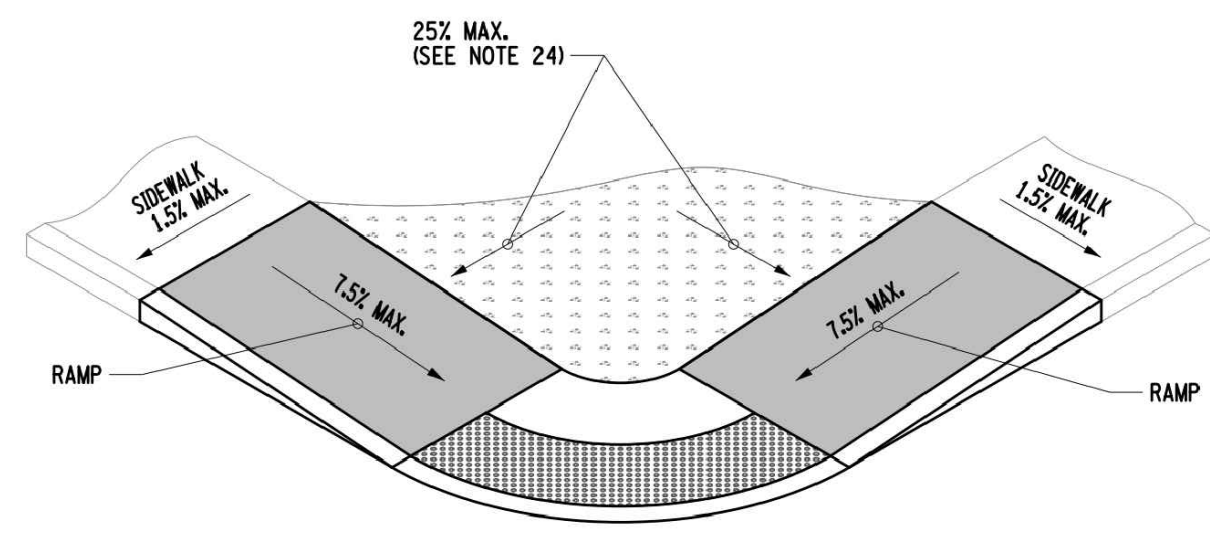
**REINFORCED CONCRETE WALKWAY DETAIL**

NOT TO SCALE  
 NOTES:  
 1. MAXIMUM CROSS SLOPE SHALL BE 1/2" PER FOOT.  
 2. 1/2" EXPANSION JOINTS SHALL BE PROVIDED AT 12' INTERVALS WITH PRE-MOLDED, BITUMINOUS JOINT FILLER, RECESSED 1/4" FROM THE SURFACE.  
 3. 1" DEEP BY 1/2" WIDE TOOLED CONTRACTION JOINTS SHALL BE PROVIDED AT 4' INTERVALS.  
 4. EXPANSION JOINT SHALL BE PROVIDED WHERE ADJACENT TO A BUILDING.

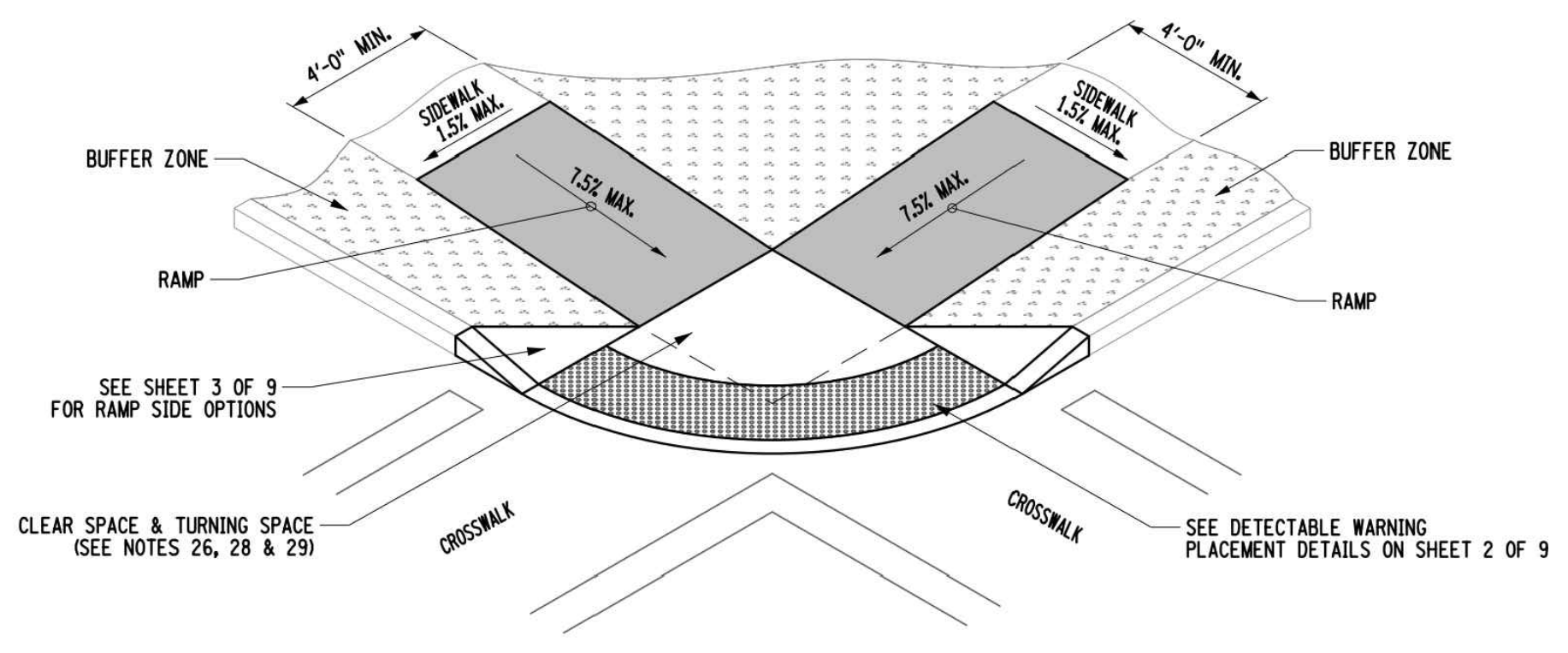


**TREE PROTECTION DETAIL**

NOT TO SCALE  
 NOTES:  
 1. SNOW FENCING IS TO BE 4'-0" HIGH AND SELF SUPPORTED.  
 2. DO NOT STOCKPILE MATERIALS OR STORE EQUIPMENT WITHIN THE TREE PROTECTION FENCING.  
 3. SNOW FENCE TO BE INSTALLED AT DRIP LINE OF EXISTING TREE OR TREE CLUSTER TO BE PROTECTED OR NO CLOSER THAN 6' FROM TREE TRUNK IF NECESSARY.  
 4. IF THE PROJECT AREA ENCOMPASSES A PORTION OF THE DRIP LINE OF THE TREE, NO MORE THAN ONE THIRD OF THE OF THE TOTAL AREA OF WITHIN THE DRIP LINE SHOULD BE DISTURBED BY CONSTRUCTION OR REGRADING AND A 3" THICK LAYER OF MULCH SHALL BE INSTALLED OVER THE AREA OF THE DRIP LINE WHICH IS NOT PROTECTED BY FENCING TO PROVIDE A CUSHION.

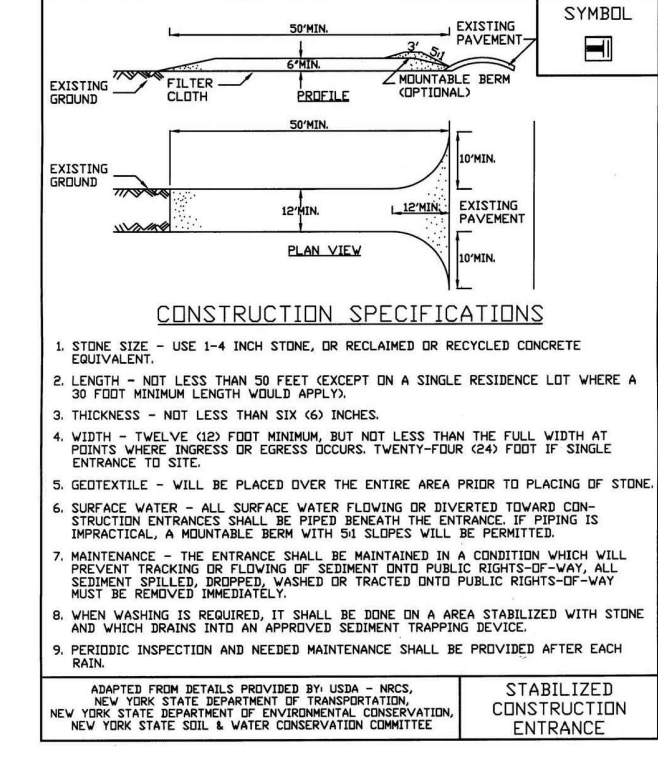


**OPTION A: GRADED EARTH AND TURF**

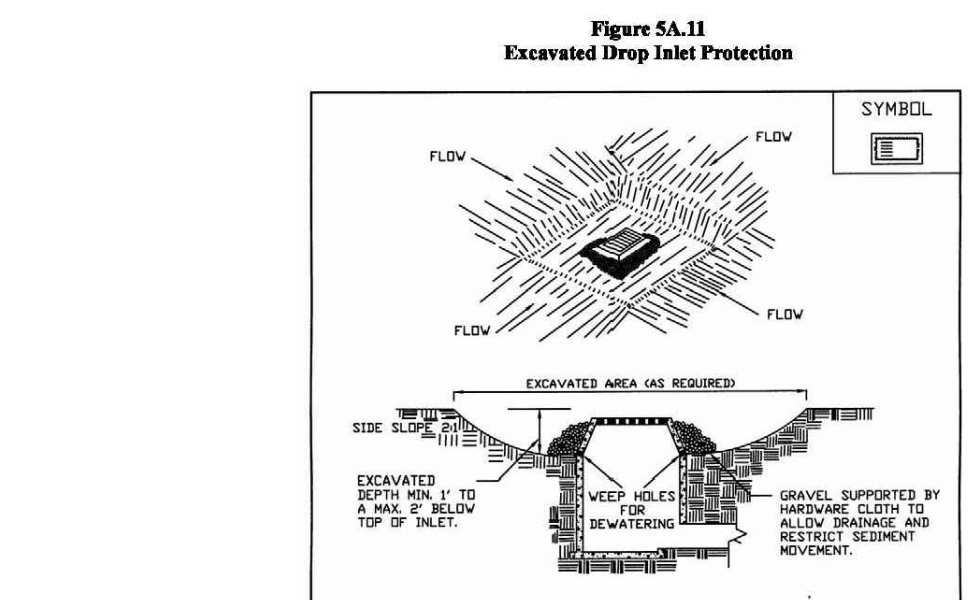


**CURB RAMP CONFIGURATION: TYPE 3**

**Figure SA.35 Stabilized Construction Entrance**

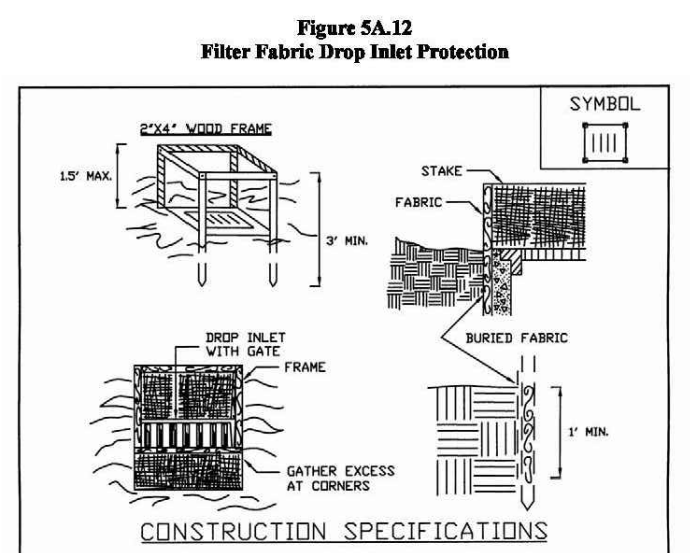


Adapted from details provided by USGA - NCSL, NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION, NEW YORK STATE SOIL & WATER CONSERVATION COMMITTEE.  
 Page SA.35 August 2005



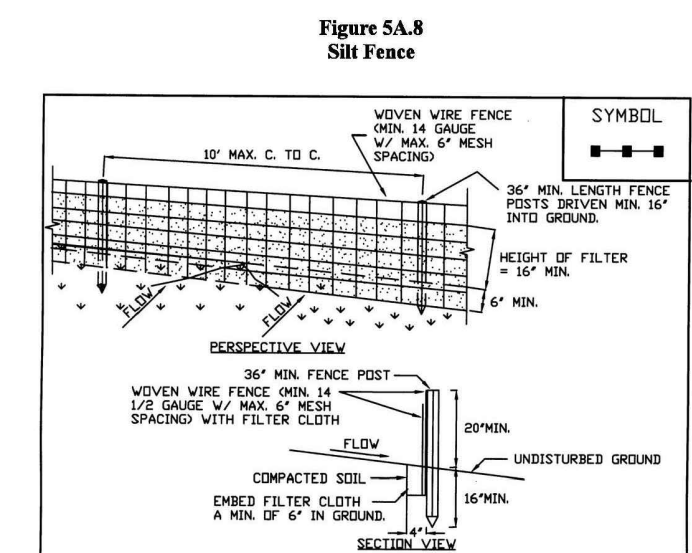
**Figure SA.11 Excavated Drop Inlet Protection**

Adapted from details provided by USGA - NCSL, NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION, NEW YORK STATE SOIL & WATER CONSERVATION COMMITTEE.  
 August 2005 Page SA.33 New York Standards and Specifications For Erosion and Sediment Control



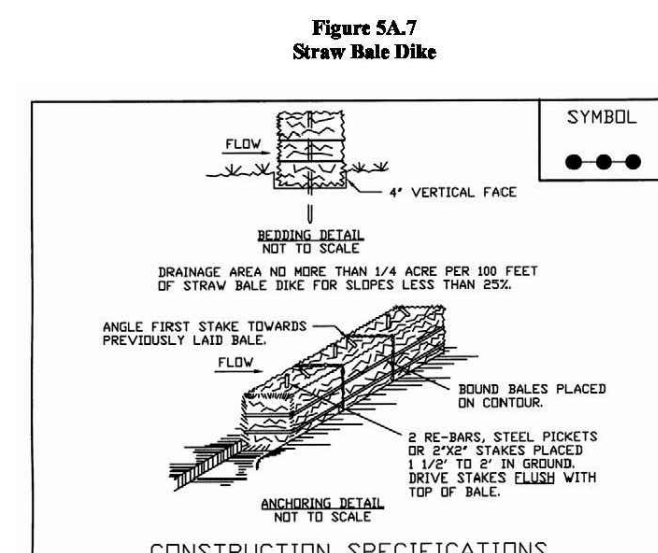
**Figure SA.12 Filter Fabric Drop Inlet Protection**

Adapted from details provided by USGA - NCSL, NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION, NEW YORK STATE SOIL & WATER CONSERVATION COMMITTEE.  
 August 2005 Page SA.30 New York Standards and Specifications For Erosion and Sediment Control



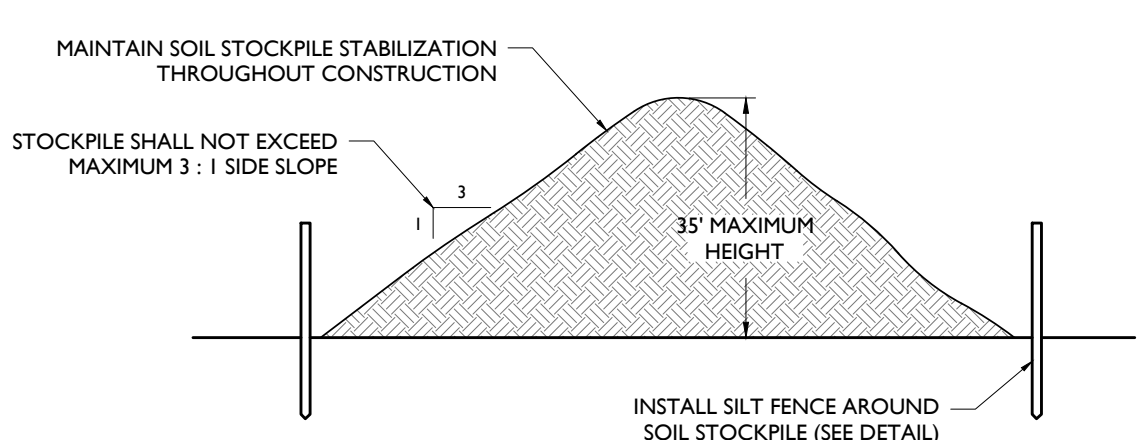
**Figure SA.8 Silt Fence**

Adapted from details provided by USGA - NCSL, NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION, NEW YORK STATE SOIL & WATER CONSERVATION COMMITTEE.  
 August 2005 Page SA.31 New York Standards and Specifications For Erosion and Sediment Control



**Figure SA.7 Straw Bale Dike**

Adapted from details provided by USGA - NCSL, NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION, NEW YORK STATE SOIL & WATER CONSERVATION COMMITTEE.  
 August 2005 Page SA.33 New York Standards and Specifications For Erosion and Sediment Control



**SOIL STOCKPILE DETAIL**

NOT TO SCALE  
 NOTES:  
 1. STOCKPILES SHALL BE SITUATED SO AS NOT TO OBSTRUCT NATURAL DRAINAGE OR CAUSE OFF-SITE ENVIRONMENTAL DAMAGE.  
 2. STOCKPILES SHALL BE STABILIZED IN ACCORDANCE WITH THE STANDARDS FOR PERMANENT OR TEMPORARY VEGETATIVE COVER FOR SOIL STABILIZATION, AS APPROPRIATE (SEE SOIL EROSION NOTES).

NO.	DATE	ISSUE	DESCRIPTION
05	01/27/2020	NS	LANDSCAPING REVISIONS PER CITY COMMENTS
04	04/04/2019	AR	RESUBMISSION AS PER NCDPW COMMENTS 02/14/19
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TITLE: CONSTRUCTION DETAILS II

DRAWING: C-8

NOTE: ALL WORK WITHIN THE NASSAU COUNTY RIGHT-OF-WAY SHALL CONFORM TO 2009 COUNTY OF NASSAU DEPARTMENT OF PUBLIC WORKS STANDARD SPECIFICATIONS AND DETAIL SHEETS FOR CIVIL ENGINEERING AND SITE DEVELOPMENT CONSTRUCTION.